

Municipal Building 625 52nd Street – Room 202	<i>Kenosha City Plan Commission</i> <i>Agenda</i>	Thursday, August 8, 2013 5:00 p.m.
<p style="text-align: center;"><i>Mayor Keith Bosman - Chairman, Alderman Kevin Mathewson - Vice-Chairman</i> <i>Alderman Anthony Kennedy, Alderman Jan Michalski,</i> <i>Kathryn Comstock, Anita Faraone, Robert Hayden, Anderson Lattimore, Jessica Olson and Ron Stevens</i></p>		

Call to Order and Roll Call

Approval of Minutes from July 18, 2013

1. Conditional Use Permit for a Recycling Collection Center to be located at 6000 49th Street. (Kenosha Recycling, Inc.) (District #16) PUBLIC HEARING
2. Conditional Use Permit for a 105,387 s.f. addition to the Carthage Straz Center at 2001 Alford Park Drive. (Carthage College) (District #1) PUBLIC HEARING
3. Conditional Use Permit for a 1,949 s.f. restaurant with a drive-thru to be located at 8040 Sheridan Road. (Taco Bell) (District #12) PUBLIC HEARING
4. By the City Plan Commission - To Repeal and Recreate Section 3.31 H. of the Zoning Ordinance for the City of Kenosha Regarding Garage Door Placement. PUBLIC HEARING
5. By the City Plan Commission - To Repeal and Recreate Section 3.32 H. of the Zoning Ordinance for the City of Kenosha Regarding Garage Door Placement. PUBLIC HEARING
6. Review of *Wisconsin Highway 50 Access Management Vision*.

Public Comments

Commissioner Comments

Staff Comments

Adjournment

Community Development & Inspections
625 52nd Street – Room 308
Kenosha, WI 53140
262.653.4030 phone / 262.653.4045 fax
www.kenosha.org

If you are disabled in need of assistance, please call 262.653.4030 at least 72 hours before this meeting.

Notice is hereby given that a majority of the members of the Common Council may be present at the meeting. Although this may constitute a quorum of the Common Council, the Council will not take any action at this meeting.

CITY PLAN COMMISSION
Minutes
JULY 18, 2013

MEMBERS PRESENT: Mayor Bosman, Alderman Mathewson, Alderman Kennedy, Alderman Michalski, Kathryn Comstock, Anita Faraone, Anderson Lattimore, Ron Stevens

MEMBERS EXCUSED: Robert Hayden, Jessica Olson

STAFF PRESENT: Jeffrey B. Labahn, Rich Schroeder, Brian Wilke and Tony Geliche

*Others Present: Alderman LaMacchia, Alderman Gordon, Alderman Prozanski
Alderman Schwartz*

The meeting was called to order at 5:01 p.m. by Mayor Bosman; the roll was called.

A motion was made and seconded to approve the minutes of June 20, 2013; the motion passed (Ayes 7; Noes 0).

Anita Faraone was sworn in by Jeff Labahn.

Alderman Mathewson arrived.

1. 2014 Community Development Block Grant Fund Allocation Plan. PUBLIC HEARING

Public hearing opened.

Tony Geliche, Community Development Specialist, stated there were no changes from the 2013 Allocation Plan and, he is available to answer any questions.

Public hearing closed.

Mayor Bosman left.

A motion was made by Alderman Michalski and seconded by Mr. Lattimore to approve Item #1. The motion passed. (Ayes 7; Noes 0)

Mayor Bosman returned.

2. By Alderman LaMacchia - To Repeal and Recreate Subparagraph 17.05 G. of the Code of General Ordinances regarding the number of lots in Final Phase Plat. PUBLIC HEARING

Public hearing opened.

Alderman LaMacchia, 2114 25th Avenue, explained that the proposed Ordinance revision would change the required minimum number of lots for a platted subdivision

from 25 to 10 for each phase, with a minimum of 6. He stated this change is necessary due to the poor economy and feels it will get more subdivisions "up and running." Alderman LaMacchia is available to answer any questions.

Public hearing closed.

Alderman Michalski asked whether the minimum number of lots is 10 or 6. Alderman LaMacchia clarified that the minimum number is 10, with 6 being the minimum for the Final Phase.

Alderman Kennedy asked if the rule for a minimum of 25 lots would be retroactive for existing subdivisions.

Rich Schroeder, Deputy Director, answered that plats for all previous subdivisions have expired. Therefore, all future plats would be required to have a minimum of 10 lots.

Alderman Kennedy asked if there are developers waiting for the Ordinance to change. Alderman LaMacchia and Mr. Schroeder answered in the affirmative.

Alderman Kennedy is concerned about one (1) possible subdivision in his district and asked what approvals are required by this Ordinance. Mr. Schroeder answered that a review by the Public Works Committee and approval by the Common Council would be required.

A motion was made by Alderman Michalski and seconded by Alderman Kennedy to approve Item #2. The motion passed. (Ayes 8; Noes 0)

3. By Alderperson Schwartz - To Repeal and Recreate Subparagraph 4.06 B.15.g. of the Zoning Ordinance regarding the Service Window at Drive-Thu Facilities. PUBLIC HEARING

Public hearing opened.

Alderperson Chris Schwartz, 402 65th Street, stated the Ordinance change would benefit downtown development, as well as other possible new development. It is mainly intended for properties with commercial use on the first floor and residential above. Hours of operation would be regulated so as not to affect residents. Alderperson Schwartz is here to answer any questions.

Public hearing closed.

A motion was made by Ms. Faraone and seconded by Alderman Michalski to approve Item #3.

Alderman Kennedy questioned how many business have been affected by this Ordinance in the past and how we got to this point. Jeffrey B. Labahn, Director, stated that the Ordinance as it currently stands has been in existence for as long as he can remember - there has not been a reason to change it until recently.

The motion passed. (Ayes 8; Noes 0)

4. Conditional Use Permit for a Tattoo Parlor to be located at 2308 63rd Street. (Kalaca Tattoo) (District #3) PUBLIC HEARING

Public hearing opened.

Abel De Los Santos, 11151 8th Avenue is available to answer questions. Mr. De Los Santos added that he will be using blinds to satisfy the requirement of shielding the operation from public view.

Public hearing closed.

Brian Wilke, Development Coordinator, explained that Mr. De Los Santos was previously approved by the Plan Commission for a tattoo parlor; but, his lease has changed and he is in a different tenant space. The new space at 2200 Roosevelt Road is zoned correctly for this use.

A motion was made by Alderman Mathewson and seconded by Alderman Kennedy to approve item #4.

Alderman Mathewson asked if Mr. De Los Santos would be putting up a wall. Mr. Schroeder answered that the plan currently shows an open concept.

Alderman Michalski is in favor of this item because he supports new business for the area. Although he feels that two (2) tattoo parlors in the uptown area will not be a problem, he will not be supporting additional businesses of this type.

Alderman Kennedy expressed approval of the open floor plan.

The motion passed. (Ayes 8; Noes 0)

Conditions of Approval
Kalaca Tattoo at 2308 63rd Street

1. The following Conditions of Approval will run with the land and shall be included in a document recorded with the Kenosha County Register of Deeds:
 - a. The applicant shall obtain all required construction permits from the Department of Community Development & Inspections. This includes, but is not limited to Building and Occupancy permits.
 - b. All signs shall comply with Chapter 15 of the Code of General Ordinances and the applicant shall obtain sign permits for all new signs. A separate submission and permits are required for all signs. Sign information provided under the Conditional Use Permit is for reference only and does not constitute sign permit approval.
 - c. The development shall be constructed per the approved building plans on file with the Department of Community Development & Inspections, Room 308, 625 52nd Street, Kenosha, WI 53140. Any changes to the approved plans shall require an amendment to the Conditional Use Permit. All changes shall be submitted to the Department of Community Development & Inspections for review and approval.

- d. Prior to the issuance of any Occupancy permits, the Conditional Use Permit shall be recorded with the Kenosha County Register of Deeds. All improvements indicated on the plans, shall be installed prior to the issuance of a final Occupancy permit. The recording fees for the Conditional Use Permit shall be submitted by the applicant.
- e. Compliance with City and State and/or Federal Codes and Ordinances. The buildings shall comply with the current Code standards in effect upon application for a building permit.
- f. All trash containers shall be stored within the enclosure or building. The applicant/owner shall be responsible for waste collection and removal for the development. The City of Kenosha shall not provide waste collection or removal services or incur any cost in this regard.
- g. The applicant shall meet all applicable Conditions of Approval and obtain a building permit within six (6) months of Common Council approval of the Conditional Use Permit or the Conditional Use Permit shall be null and void.
- h. Outdoor display of products is prohibited.
- i. All vehicles shall be parked within the designated paved areas.
- j. All improvements, including landscaping, shall be maintained per the approved plans. Any damaged fencing, landscaping or building shall be replaced or reconstructed per the approved plans.
- k. Compliance with the Operational Plan dated June 25, 2013.

5. Conditional Use Permit for a Recycling Collection Center to be located at 6000 49th Street. (Kenosha Recycling, Inc.) (District #16) PUBLIC HEARING

Public hearing opened.

Mike Goffman, 6000 49th Street, is here to answer questions, particularly regarding fencing.

Public hearing closed.

Mr. Schroeder showed a video regarding the site and informed the Commission that Mr. Goffman submitted a different Site Plan within the last few days. Also, Mr. Goffman has already started moving items on to the site. Mr. Schroeder suggested the item be deferred until Mr. Goffman has a better idea of what he is going to do.

A motion to defer the item was made by Alderman Mathewson and seconded by Alderman Michalski.

Ms. Faraone asked if Mr. Goffman is allowed to move any items on to the site before the Conditional Use Permit is approved. Mr. Schroeder answered in the negative.

Alderman Michalski informed the Commission that the Alderman of the District, Jesse Downing, asked that the Commission vote the item down. Alderman Michalski urged

Mr. Goffman to talk with Alderman Downing.

Alderman Kennedy concurred with the deferral and agreed that the submittal of plans that are contrary to what had been documented and given to the Commission shows that his intentions are unclear.

The motion to defer Item #5 until the next meeting passed. (Ayes 8; Noes 0)

Mayor Bosman asked that Item #6 and Item #7 be taken together for public hearing purposes. The items were read.

- 6. Conditional Use Permit for a 192,940 s.f. grocery/retail store and a 2,509 s.f. gas station/convenience store to be located at 7701 Green Bay Road. (Meijer) (District #14) PUBLIC HEARING**
- 7. By the Mayor - Resolution to approve a three-lot Certified Survey Map to be located at 7701 Green Bay Road. (Meijer) (District #14) PUBLIC HEARING**

Public hearing opened.

Brian Randall, 330 East Kilbourn Avenue, Milwaukee, Wisconsin gave a brief history and overview of Meijer's and spoke regarding their outreach to the community. He is available to answer any questions.

Dave Behrens, 21 S. Evergreen Avenue, Arlington Heights, Illinois, spoke. He stated he has worked extensively with City Staff, as well as with the Department of Natural Resources (DNR) and the Department of Transportation (DOT). Many site layouts and building elevations were reviewed. Additional land was purchased due to the fact that the DOT required public road connections. Parking spaces were eliminated and more green space was added per City Staff's recommendation. The limestone exterior of the building and the green space reflect the character of Kenosha.

Michael May, N 36 W7505 Buchanan Street, Cedarburg, Wisconsin, Traffic Engineer for Traffic Analysis & Design, Inc. (TADI), has been working with the DOT regarding acceptance of the plans, specifically the access points at 78th Street and 76th Street. He is available to answer questions.

Alderman LaMacchia voiced support for the project and stated the City of Kenosha is "landlocked" and the only way to expand is going west. He said everyone is for progress; but nobody wants it in their back yard. The Meijer project will provide jobs.

Roger Clark, Plumbers and Pipe Fitters Union, 3030 39th Avenue, said the project would create jobs for area contractors.

Gerald Rintamaki, International Union of Painters and Allied Trades, 29007 Bushnell Road, Burlington, Wisconsin stated the project is a "godsend" for workers in the Drywall Finishers' Union.

Lieutenant Jason Alcomm, Salvation Army, 8611 36th Avenue, said he moved to Kenosha on June 26 from Michigan. He was impressed by the Meijer stores in

Michigan and stated the Salvation Army benefited from donations of produce and other items.

Michael Garcia, 5822 69th Street, representing Electrical Workers Union Local 127, is in support of Meijer's because it will mean jobs for local workers. He mentioned that Festival Foods has employed non-local workers.

Russell Guerra, 2009 64th Street, representing Electrical Workers Union Local 113, supports the Meijer project and would like to see local contractors get back to work.

Dan Doperalski, 3720 Republic Avenue, Racine, from the Plasterers & Cement Masons Local 599, supports the Meijer project. He is impressed by Meijer's existing stores and hopes this project will put local trades to work.

Rollin Pizzala, 5303 43rd Avenue, spoke in support of the project. Mr. Pizzala stated he has made deliveries at various Meijer's stores and has always been treated well. Mr. Pizzala also mentioned that their security is very good - they have an employee at each entrance at all times.

Keith Kemple, 24130 Church Road, Kansasville, Sheet Metal Workers, stated that he heard that local contractors were hired for Meijer's stores in Michigan and they were treated very well.

Kent Cramer, Electrical Workers Union Local 494, 3303 103rd Street, Milwaukee, stated the Meijer project should be supported and believes more stores will be built in Wisconsin.

Robyn Woods, 6907 78th Street, believes the Meijer's store will have a negative impact on Kenosha's economy, increase the cost of maintaining police and fire, add noise pollution, increase the criminal element, and be a threat to other local businesses.

Dan Vukovich, 8137 S. White Oak, spoke in support of the Meijer project. Mr. Vukovich feels Meijer's is a quality company who would not be a burden to taxpayers, as they pay for their own fire inspections, garbage pick-up, and snow-plowing. Mr. Vukovich also stated that Kenosha is blessed to be chosen as the site for their new store and that Meijer's is not only creating temporary jobs during construction, but permanent jobs as well.

Virginia Hoekstra, 6209 75th Street, stated she is not against Meijer's - she is against the location and would rather it be located downtown or at the old Chrysler site.

Sarah Billings, 5903 83rd Street, said she lives in the area of the proposed Meijer's store and is in support of the project. Ms. Billings pointed out that if Meijer's felt there was a better location, they would build the store there.

Diane White, 3919 Prairie Village Drive, Pleasant Prairie, stated she works for a local retailer and is concerned about how many jobs will be lost at other stores.

Joe Cincatta, 757 N. Broadway, Milwaukee, Attorney for Virginia Hoekstra, spoke

against the Meijer project, saying that this type of store does not belong in this area. He does not feel the economic study addressed all of the issues that were supposed to be addressed.

Alderman Scott Gordon, 5204 46th Avenue, asked the Commission to approve the Meijer project. Alderman Gordon stated he received minimal objections from his constituents. Alderman Gordon feels the new store may draw people from Illinois and they may visit other establishments in Kenosha while they are here.

Sharon Ingham, 6912 78th Street, is concerned about drainage. Ms. Ingham said she has been in contact with City Staff regarding her concerns about the possibility of the storm water that currently flows west from the site through the easement on her property to the Des Plaines River becoming worse. Mayor Bosman encouraged her to continue to be in contact with City Staff.

Robert Avvisati, 616 Stafford, Roselle, Illinois, Manager of Pick-n-Save on 75th Street, Kenosha, stated that the number of grocery stores should be limited; as his store has already been forced to cut back on staff.

Jim Anderson, 4127 Wilson Road, supports the Meijer's project for jobs it will create.

Alderman Daniel Prozanski, Jr., 4429 88th Place, Alderman of the 14th District which contains the site, commended Meijer for the way they are handling the proposed project, they contacted him early in the process and he has been at all their public sessions and listened to all the concerns. Alderman Prozanski has walked along the Drainage Easement, as has Staff from Public Works. Currently the water flows west unrestricted. The retention ponds may reduce the rate of flowage. Alderman Prozanski stated that a significant amount would be added to the City's tax base if the Meijer store is built - approximately \$480,000, including \$195,133.95 for the City. Alderman Prozanski received many e-mails from area residents in support of Meijer's and believes that the members of Kenosha Residents Against Meijer's (KRAM), who had placed ads in the Kenosha News, and some of whom spoke earlier, are in the minority. Alderman Prozanski also pointed out that Attorney Cincotta, who represents KRAM member Virginia Hoekstra, has represented other citizens in other areas where Meijer's stores have been built.

Public hearing closed.

Mayor Bosman thanked all who came to speak on the items.

Mr. Wilke showed a video of the site. Mr. Wilke said that four (4) single-family homes have been purchased and razed on the site.

Mr. Schroeder said if the items are approved by the Plan Commission, they would go to Common Council on August 5. Mr. Schroeder said representatives from Public Works are here to answer questions regarding drainage.

Ms. Faraone made a motion to approve Item #6 subject to conditions; Alderman Michalski seconded the motion.

Alderman Michalski said the plans Meijer has submitted are beautiful and he is happy with the mitigation of the wetland area.

Ms. Faraone asked whether or not Meijer's meets the criteria of a large-scale development. Mr. Wilke answered that they do meet the criteria.

Ms. Faraone asked if Meijer's is using local contractors. Mr. Randall answered that he intends to put the jobs out for bid and they will be made available to local contractors.

Ms. Faraone expressed support of the project, stating it would be good for the community as a whole and would bring people in from other areas. Ms. Faraone added that it appears that the zoning requirements and other criteria have been met. Ms. Faraone feels that there is no question that the Commission shows due diligence when considering these matters.

Alderman Mathewson noted that Meijer has not provided an average wage for their employees. Mr. Randall answered that they are a private company, and as such do not share that information when there is a chance it may become public record.

Alderman Mathewson asked about the timing of the deliveries. Mr. Labahn answered that those types of limitations are typically addressed in the Conditional Use Permit. It may be different if the site is surrounded by other commercial properties rather than residential.

Alderman Mathewson voiced support of the project; but, requested an overview of the changes in the drainage.

Jeff Hanson, Public Works, explained that water currently flows unrestricted west through a pipe on the northwest corner of the property. When the retention ponds are added, they will restrict the rate at which water flows through the pipes. The water will flow for a longer period of time, but will flow more slowly.

Alderman Mathewson asked if Meijer's is receptive to the required plan. Mr. Hanson answered that he has no doubt they can come to an agreement.

Alderman Mathewson then inquired about the reduction of parking spaces. Mr. Wilke answered that the number will be reduced from the required 775 spaces to 706 spaces. The Zoning Ordinance can provide relief through a Special Exception Request. Alderman Mathewson asked if 706 spaces is adequate. Mr. Wilke answered in the affirmative.

Alderman Mathewson asked if the trees cut down during construction would be replaced. Mr. Wilke said all trees would be replaced.

Alderman Mathewson asked for clarification regarding public displays. Mr. Wilke answered it is typical in the City of Kenosha to not allow merchandise to be displayed on the exterior premises.

Alderman Mathewson is not concerned about loss of sales - a free market results in better retail prices.

Ms. Comstock supports the project because Kenosha needs jobs. She also feels that competition is good - its what our country was built on.

Per request from Mr. Lattimore, Lynn Broderick, 6696 Kingsway Drive, Cedarburg, Wisconsin gave examples regarding how Meijer's reaches out to the Community, such as providing help to schools, food pantries, and playgrounds for children with special needs.

Mayor Bosman commended Meijer's for the work they have done to this point. He stated that the nice-looking building, as well as the additional green space, will speak well for Kenosha.

Alderman Michalski advised Meijer's to consider the large number of people from the building trades who "went to bat" for them today when it is time to hire contractors.

Item #6, the Conditional Use Permit passed .(Ayes 8; Noes 0)

Conditions of Approval
Meijer's at 7701 Green Bay Road

1. The following Conditions of Approval will run with the land and shall be included in a document recorded with the Kenosha County Register of Deeds:
 - a. The applicant shall obtain all required construction permits from the Department of Community Development & Inspections. This includes, but is not limited to Raze, Erosion Control, Building, Plumbing, Electrical and Occupancy permits.
 - b. The applicant shall obtain Driveway, Sidewalk, Street Opening and Parking Lot permits from the Department of Public Works. Any work within the STH 31 or STH 50 rights-of-way will require permits from the State Department of Transportation.
 - c. All signs shall comply with Chapter 15 of the Code of General Ordinances and the applicant shall obtain sign permits for all new signs. A separate submission and permits are required for all signs. Sign information provided under the Conditional Use Permit is for reference only and does not constitute sign permit approval. This site is considered a Unified Business Center and is therefore subject to the signage rules for a Unified Business Center.
 - d. The development shall be constructed per the approved site, drainage, utility, landscape and building plans on file with the Department of Community Development & Inspections, Room 308, 625 52nd Street, Kenosha, WI 53140. Any changes to the approved plans shall require an amendment to the Conditional Use Permit. All changes shall be submitted to the Department of Community Development & Inspections for review and approval.
 - e. Prior to the issuance of any Occupancy permits, all parking areas, drives and designated paved areas shall have the initial lift of asphalt installed. The building exterior shall be completed per the approved plans, the exterior lighting shall be installed and the Conditional Use Permit shall be recorded with the Kenosha County Register of Deeds. All improvements indicated on the plans, including landscaping, shall be installed prior to the issuance of a final Occupancy permit. The recording fees for the Conditional Use Permit shall be submitted by the applicant.

- f. Compliance with City and State and/or Federal Codes and Ordinances and the Developer's Agreement. The buildings shall comply with the current Code standards in effect upon application for a building permit. The Developer's Agreement shall be approved and executed and all required assurances posted prior to the issuance of any construction permits.
 - g. All roof top mechanicals shall be properly screened per Section 14.07 B.10 of the Zoning Ordinance.
 - h. All trash containers shall be stored within the enclosure or building. The applicant/owner shall be responsible for waste collection and removal for the development. The City of Kenosha shall not provide waste collection or removal services or incur any cost in this regard.
 - i. Applicant shall meet all applicable Conditions of Approval and obtain a permit for one building within six (6) months of Common Council approval. Permits for all other buildings shall be obtained within two (2) years of the Common Council approval or the Conditional Use Permit shall be null and void.
 - j. Outdoor display of products is prohibited.
 - k. All vehicles shall be parked within the designated paved areas.
 - l. All improvements, including landscaping, shall be maintained per the approved plans. Any damaged fencing, landscaping or building shall be replaced or reconstructed per the approved plans.
 - m. Cross access shall be provided to adjacent parcels if required at a future date by the City.
 - n. Compliance with the Operational Plan dated May 6, 2013.
 - o. Storm water detention basins shall be constructed and operational prior to the issuance of a Footing and Foundation permit.
 - p. All applicable public improvements shall be completed prior to the issuance of any occupancy permits.
 - q. All Wisconsin Department of Natural Resources approvals and permits shall be obtained prior to the issuance of any construction permits.
2. The following conditions of approval shall be satisfied with City Staff prior to the issuance of any construction permits.
- a. The Drainage Plan shall be revised and resubmitted for review and approval to address the following comments:
 - 1. All storm sewer conveying water from the public street is to be public storm sewer and is to be either within the right of way of the public street or within an easement with a minimum width of 20'. Easements are also required to provide access to the inlet and outlet structures in the proposed detention ponds. The landscape plan should avoid the placement of shade trees within the storm sewer easements.
 - 2. The owner is to provide the storm sewer and detention pond easements to the pavements or landscaping or other facilities during City maintenance (i.e. if the

- City needs to drive heavy equipment across the parking lot to provide maintenance to the public sewer the City is not responsible for any restoration, even if the truck needs to drive outside of the established easement). This language should also be included in the stormwater maintenance agreement.
3. Label all storm sewer on the utility plan as "private" or "public".
 4. All public storm sewer and pipes are to be constructed to City of Kenosha standards using City details. HDPE pipe will be allowed in areas outside of the public street.
 5. Per City of Kenosha policy, no inlets or storm sewer structures shall have sumps.
 6. The storm sewer main line between STM CB263 and STM CB 35 should be located either just behind the rec trail or in the street for access purposes. If the line is located in the street, the pipe must be reinforced concrete.
 7. Revise the storm sewer alignment so that a line runs from either STM CB2 or a structure just outside of the pavement near STM CB2 to either STM CB7 or a structure just outside of the pavement near STM CB7. The public storm sewer is not to be routed through CB3, CB4, or CB8.
 8. The maximum allowable slope in and around the detention ponds is 4:1.
 9. The minimum berm width around the detention ponds is 10'.
 10. The northeast pond contains a small area just east of driveway to 64th Avenue that has 3' of permanent pool depth. A minimum of 4' of water depth should be included in all wet portions of the pond. Retaining walls are allowed to be used in pond areas if needed to achieve the required storage area for the pond while meeting the City's required maximum slopes, minimum berm width, and minimum water depth requirements.
 11. The landscaping plan shows wildflowers to be planted to 1' below the permanent pool elevation of each pond.
 12. Provide more information on the proposed plantings to be located below the secondary outlet orifice of each pond. Plantings should be able to survive partially or fully submerged as the pond draw down time is upwards of 7 days due to the outflow restrictions of the Des Plaines River watershed.
 13. The 18" and 24" pipes that drain water from each detention pond to the main outlet structures require trash grates that are a minimum of 4 times the area of the pipes. Riprap should also be placed surrounding each of the endwalls.
 14. The plans propose to use a "Snout Oil and Debris Separator" to prevent debris from clogging the 1.5" bottom orifice of each pond's outlet structure. If clogging of the bottom orifice is a problem when the pond is in operation, the City reserves the right to have the owner replace the device with an alternate method of preventing clogging.
 15. Provide a detail of the proposed anti-seep collars and show on plans where they are proposed to be located.
 16. Per the City of Kenosha's stormwater criteria, the emergency overflow of each pond is to be either poured concrete, or an 18" thick layer of riprap buried under a thin layer of topsoil with a turf reinforcement mat above.
 17. The landscape plan needs to be revised to remove the proposed trees in the areas of the emergency overflow for each pond
 18. The plans call out for "American Green C350 Permanent Turf Reinforcement Mat". Per the Wisconsin DOT's Product Acceptability List, the American Green C350 is an "Erosion Control Revegetative Mat" listed as a Class II type B mat. Permanent turf reinforcement mats are listed as Class III Type B or greater. Also, show on the plans the extent of the proposed turf reinforcement mat.
 19. The detail of the outlet structure of each pond contains a top of concrete weir elevation that look unworkable. For the northeast pond, the top of concrete weir is 1' below the rim of the structure. It appears that the height of the proposed casting and the top slab of the structure would be more than 1'. The

City suggests lowering the weirs to just over the 100-yr elevation of each pond, provided that allows for enough concrete over the 10" orifices to make the weir stable.

20. The proposed wetland mitigation area contains no bottom outlet. There are concerns about the mitigation area holding water at elevation 699.5. This would reduce the amount of storage area in the mitigation area and have a negative impact on the abutting residential property. It would also pool water just behind the sidewalk along Green Bay Road and then flow over the sidewalk during rain events. Provide an outlet structure with a bottom orifice elevation of 699.0 that connects to the storm sewer. Also provide the ENCAP plans for the mitigation area. If these requirements conflict with DNR or Army Corps requirements for the mitigation area please contact Jeff Hansen to discuss.
21. Clarify if the northwest pond will require a DNR Chapter 31 permit due to the pond being constructed over an existing wetland.

Traffic Comments:

1. Install No-Parking signs along the public street to prohibit parking on both sides of the street.
 2. Install ramps with detectable warning fields (natural patina) at all pedestrian/handicap accessible entrances to the building. Provide details for these ramps.
 3. Install sidewalk along Green Bay Road.
 4. Provide ramps and detectable warning fields on all island and medians on which there is a pedestrian path.
 5. Provide sidewalk cross-section details. Sidewalk shall be 4-inch thick over at least 2-inch dense aggregate.
 6. Sidewalk must continue through all driveways.
 7. North and southwest main driveways must have detectable warning fields on both sides.
 8. The asphalt multi-use trail shall be 4-inch of asphalt pavement over 4-inch of 1 ¼" dense aggregate. Consider using 6-inch concrete pavement over 2-inch aggregate if it is more economical.
 9. The public street pavement shall be 8-inch of asphalt or concrete pavement over 6 -inch of 1 ¼" dense aggregate.
 10. Provide street cross-sections and profile for the public street.
 11. We recommend concrete pavement at the loading dock area.
- b. The Utility Plan shall be revised and resubmitted for review and approval addressing the comments listed in the Kenosha Water Utility Memo dated June 20, 2013.
 - c. A Land Division shall be recorded prior to the issuance of a Footing and Foundation permit.
 - d. An exception to the number of off-street parking spaces required shall be approved by the Common Council.
 - e. The right side (west) and rear (south) elevations of the gas station must meet the minimum sixty percent (60%) articulation requirement for elevations that abut a public street.
 - f. A Tree Protection Permit is required for the residentially used properties on the Southwest corner of the development.

- g. The Site Plan shall show additional bike rack near the northern main entrance of the building and a bike rack near the convenience store.

A motion was made by Alderman Kennedy and seconded by Alderman Michalski to approve Item #7, the Certified Survey Map. The motion passed. (Ayes 8; Noes 0)

Conditions for Certified Survey Map
7701 Green Bay Road

1. Compliance with all applicable State and City codes and ordinances.
2. Payment of all applicable fees, including recording fees.
3. Payment of all taxes, special assessments and storm water utility fees prior to recording. The Owner shall provide the City Clerk-Treasurer, a certificate from the County Treasurer, stating that there are no past-due real estate taxes or special assessments on the parcel of real estate which is being divided.
4. The Certified Survey Map shall be null and void if not recorded within six (6) months of approval by the Common Council.
5. A digital copy of the Certified Survey Map shall be provided to the City prior to recording.
6. Sheets 4-7 incorrectly indicate a "CSM 33879" on the bottom of each page. Please revise.
7. Sheets 1 & 4 refer to the Chicago & Northwestern Railroad. This is currently owned by Union Pacific.
8. Sheet 6 indicates, Ocean Spray Cranberries certifies, "...in accordance with the ordinances of the Village of Mount Pleasant." Please indicate that the ordinances belong to the City of Kenosha.
9. There is additional discontinuous land to be dedicated for 76th Street (Dedication Detail 1; sheet 3). Since neither Meijer Stores Limited Partnership nor Ocean Spray Cranberries, Incorporated, is the owner of record for this property; an additional Owner's Certificate sheet must be included to indicate the assent of said property owner, or the land dedicated for 76th Street should be removed from the Certified Survey Map.
10. Sheets 1 & 2 show a series of curves notated as C9, C10 & C11. When replicating the line work in CAD, C9 does not constitute the combination of C10 & C11. If one substitutes C9 for C11, lines L38 & L39 fall into their proper places. Please correct the curve table.
11. Label the north/south portion of the new public road as 63rd Avenue.
12. Compliance with all the preceding conditions as a prerequisite for authorizing Mayor and City Clerk-Treasurer to sign the Certified Survey Map.
13. Applicant shall enter into a developer's agreement with the City and Kenosha Water Utility and submit all required exhibits prior to recording the Certified Survey Map.

Public Comments

No public comments.

Commissioner Comments

No Commissioner comments.

Staff Comments

A motion to adjourn was made by Alderman Michalski and seconded by Mr. Lattimore. The motion passed. (Ayes 8; Noes 0) The meeting adjourned at 7:17 p.m.

Minutes Prepared by: Sue Zampanti, Department of Community Development & Inspections

Planning & Zoning Division 625 52nd Street - Room 308 Kenosha, WI 53140 262.653.4030	Kenosha City Plan Commission FACT SHEET	August 8, 2013	Item 1 Page 1
Conditional Use Permit for a Recycling Collection Center to be located at 6000 49th Street. (Kenosha Recycling, Inc.) (District #16) PUBLIC HEARING			

LOCATION/SURROUNDINGS:

Site: 6000 49th Street
 Zoned: M-2 Heavy Manufacturing / AIR-4 Airport Overlay District

NOTIFICATIONS/PROCEDURES:

The alderman of the district, Alderman Downing, has been notified. The Common Council is the final review authority.

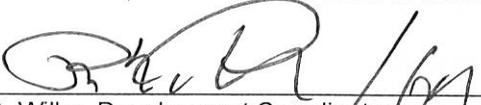
ANALYSIS:

- The applicant is requesting a Conditional Use Permit to operate a Recycling Collection Center in the existing building and site at 6000 49th Street. The use would have to be licensed under Chapter 13 of the Code of General Ordinances. The Common Council approves the license.
- The proposal includes occupying roughly half of the existing building space as well as exterior storage of roll-off boxes and a truck scale. The applicant will be required to screen the site with a site-obscuring fence and a landscaped berm. Given the high visibility of the site, Staff feels that extensive screening is necessary along Green Bay Road and 49th Street. All areas where vehicles are parked or travel will have to be paved with asphalt or concrete.
- According to the operational plan, the primary business activity is recycling of all ferrous and nonferrous metals. The site would also accept used building materials.
- This plan was deferred by the City Plan Commission at their meeting on July 18, 2013 since the applicant submitted conflicting plans prior to the meeting that Staff had not reviewed. Also, the applicant had moved several pieces of equipment onto the site, including roll-off boxes and truck scale. Staff and the City Plan Commission requested the applicant to meet with the Alderman of the District and Staff to discuss the project.
- Alderman Downing met with the applicant and shared his concern with Staff that he desires a highly and effectively screened site given the site's prominent location.
- If the Conditional Use Permit is approved, Staff will require all the site paving and landscaping/fencing to be installed prior to the business opening. A copy of potential Conditions of Approval is attached if the Commission moves to approve the Conditional Use Permit.
- Plans have been sent to City Departments for their review. Their comments have been included in the attached conditions of approval. The applicant has submitted a revised Site Plan.
- The Plan submitted is incomplete and does not clearly show how the operation will function. Staff is concerned that materials have been moved onto the site without any approvals.

<i>Planning & Zoning Division 625 52nd Street - Room 308 Kenosha, WI 53140 262.653.4030</i>	<i>Kenosha City Plan Commission FACT SHEET</i>	<i>August 8, 2013</i>	<i>Item 1 Page 2</i>
<i>Conditional Use Permit for a Recycling Collection Center to be located at 6000 49th Street. (Kenosha Recycling, Inc.) (District #16) PUBLIC HEARING</i>			

RECOMMENDATION:

Since the plans are incomplete and a determination cannot be made as to if the application will comply with City Ordinances, a recommendation is made to deny the Conditional Use Permit.



Brian R. Wilke, Development Coordinator
[/u2/acct/cp/ckays/1CPC/2013/AUG8/fact-cup-keno-recycle.odt](#)

Jeffrey B. Labahn, Director

Planning & Zoning Division
625 52nd Street
Kenosha, WI 53140
262.653.4030

***Kenosha City Plan
Commission
Conditions of Approval***

**Kenosha Recycling, Inc.
6000 49th Street**

August 8, 2013

1. The following Conditions of Approval will run with the land and shall be included in a document recorded with the Kenosha County Register of Deeds:
 - a. The applicant shall obtain all required construction permits from the Department of Community Development & Inspections. This includes, but is not limited to Building and Occupancy permits.
 - b. The applicant shall obtain any applicable permit from the Department of Public Works.
 - c. All signs shall comply with Chapter 15 of the Code of General Ordinances and the applicant shall obtain sign permits for all new signs. A separate submission and permits are required for all signs. Sign information provided under the Conditional Use Permit is for reference only and does not constitute sign permit approval. Only one monument sign is permitted for the site.
 - d. The development shall be constructed per the approved building plans on file with the Department of Community Development & Inspections, Room 308, 625 52nd Street, Kenosha, WI 53140. Any changes to the approved plans shall require an amendment to the Conditional Use Permit. All changes shall be submitted to the Department of Community Development & Inspections for review and approval.
 - e. Prior to the issuance of any occupancy permits, all parking areas, drives and designated paved areas shall have the initial lift of asphalt installed. The landscape berm and fencing shall be installed prior to any Occupancy Permits. The Conditional Use Permit shall be recorded with the Kenosha County Register of Deeds. All other improvements indicated on the plans, shall be installed prior to the issuance of a final Occupancy permit. The recording fees for the Conditional Use Permit shall be submitted by the applicant.
 - f. Compliance with City and State and/or Federal Codes and Ordinances. The buildings shall comply with the current Code standards in effect upon application for a building permit.
 - g. All trash containers shall be stored within the enclosure, building or a screened area of the site. The applicant/owner shall be responsible for waste collection and removal for the development. The City of Kenosha shall not provide waste collection or removal services or incur any cost in this regard.
 - h. The applicant shall meet all applicable Conditions of Approval and obtain a building permit within six (6) months of Common Council approval of the Conditional Use Permit or the Conditional Use Permit shall be null and void.
 - i. Outdoor display or storage of products is prohibited, except within the storage bins.

Planning & Zoning Division 625 52nd Street Kenosha, WI 53140 262.653.4030	<i>Kenosha City Plan Commission Conditions of Approval</i>	Kenosha Recycling, Inc. 6000 49th Street	August 8, 2013
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- j. All vehicles shall be parked within the designated paved areas.
 - k. All improvements, including landscaping, shall be maintained per the approved plans. Any damaged fencing, landscaping or building shall be replaced or reconstructed per the approved plans.
 - l. Compliance with the operational plan dated June 20, 2013.
 - m. Prior to commencing operations at the site, the applicant shall obtain a Recycling Center Activity License from the Common Council, subject to Chapter 13 of the Code of General Ordinances.
 - n. The material pile heights shall not exceed the height of the fence.
2. The following conditions of approval shall be satisfied with City Staff prior to the issuance of any Construction permits.
- a. A site-obscuring wood or vinyl fence shall be installed around the perimeter of the site along Green Bay Road, 49th Street and along the north lot line from Green Bay Road to a line equal to the front of the building directly to the north. Also, a landscaped berm a minimum of six feet (6') in height shall be installed to screen the site along Green Bay Road and 49th Street. The berm shall not be located within the major street setback. Any fencing along the south property line shall not be located within the 49th Street right-of-way.
 - b. All areas used for vehicular parking or movement and materials storage areas shall be paved with asphalt or concrete.
 - c. The comments in the Public Works memo dated July 1, 2013 shall be addressed.
 - d. A landscaped plan shall be submitted which complies with Section 14.0 of the Zoning Ordinance. Specifically, landscaping shall be installed in addition to the fence and berming to screen the site.
 - e. Sidewalks shall be installed along the entire Green Bay Road and 49th Street lot frontage.
 - f. The existing chain-link and barbed wire fence shall be removed.



ENGINEERING DIVISION
 SHELLY BILLINGSLEY, P.E.
 CITY ENGINEER

PARK DIVISION
 JEFF WARNOCK
 SUPERINTENDENT

FLEET MAINTENANCE
 MAURO LENCI
 SUPERINTENDENT

STREET DIVISION
 JOHN H. PRIJIC
 SUPERINTENDENT

WASTE DIVISION
 ROCKY BEDNAR
 SUPERINTENDENT

DEPARTMENT OF PUBLIC WORKS

MICHAEL M. LEMENS, P.E., DIRECTOR
 SHELLY BILLINGSLEY, P.E., DEPUTY DIRECTOR

MUNICIPAL BUILDING · 625 - 52ND ST · RM 305 · KENOSHA, WI 53140
 TELEPHONE (262) 653-4050 · FAX (262) 653-4056
 EMAIL PUBLICWORKS@KENOSHA.ORG

TO: Brian Wilke, Development Coordinator

FROM: Michael M. Lemens, P.E.
 Director of Public Works

Shelly Billingsley, P.E.
 City Engineer

[Handwritten signatures and initials]
 7-10-13

DATE: July 1, 2013
SUBJECT: PLAN REVIEW COMMENTS
Project Description: Kenosha Recycling Inc.
Location: 6000 49th Street

Our staff has reviewed the plans for this project. The following comments are provided:

Parking Lot Ordinance Compliance	Sufficient	Deficient	Not Applicable
Parking Lot Paved		X	
Standard Stall Width		X	
Parking Lot Layout		X	
Parking Lot Lighting Shown		X	
Parking Lot Lighting Adequate		X	
Handicapped Parking		X	
Driveway Locations		X	
Driveway Width		X	
Passing Blister or Accel/Decel Lanes		X	
Sidewalks Adequate		X	
Drive Thru Lane Design		X	

Public Streets	Sufficient	Deficient	Not Applicable
Geometric Design			X
Pavement Width			X
Pavement Thickness Design			X
Established Grades			X
Plan Details			X
Sidewalks			X
Street Lights			X

Site Grading/Drainage	Sufficient	Deficient	Not Applicable
Drainage Plan		X	
Storm Sewer		X	
Storm Water Detention		X	
Drainage Calculations		X	

Project Approval/Permits Needed	Yes	No	Not Applicable
Project Approved for Permitting		X	
Withhold Permits: See Comments	X		
Approve Footing/ Foundation Only (per condition)			X
Parking Lot Permit Required			
Driveway Permits Required			
Sidewalk Permit Required			
Street Opening Permit Required			
Stormwater Permit Required			
Erosion Control Required	X		
State Permit Required			

Grading & Drainage Comments:

- Existing and proposed site plans are required to adequately review the site. Plans should show the existing and proposed pervious areas and the proposed area of disturbance so it can be determined if post-construction stormwater management facilities will be required.
- Further comments will be given when a full set of plans are reviewed.

Traffic Comments:

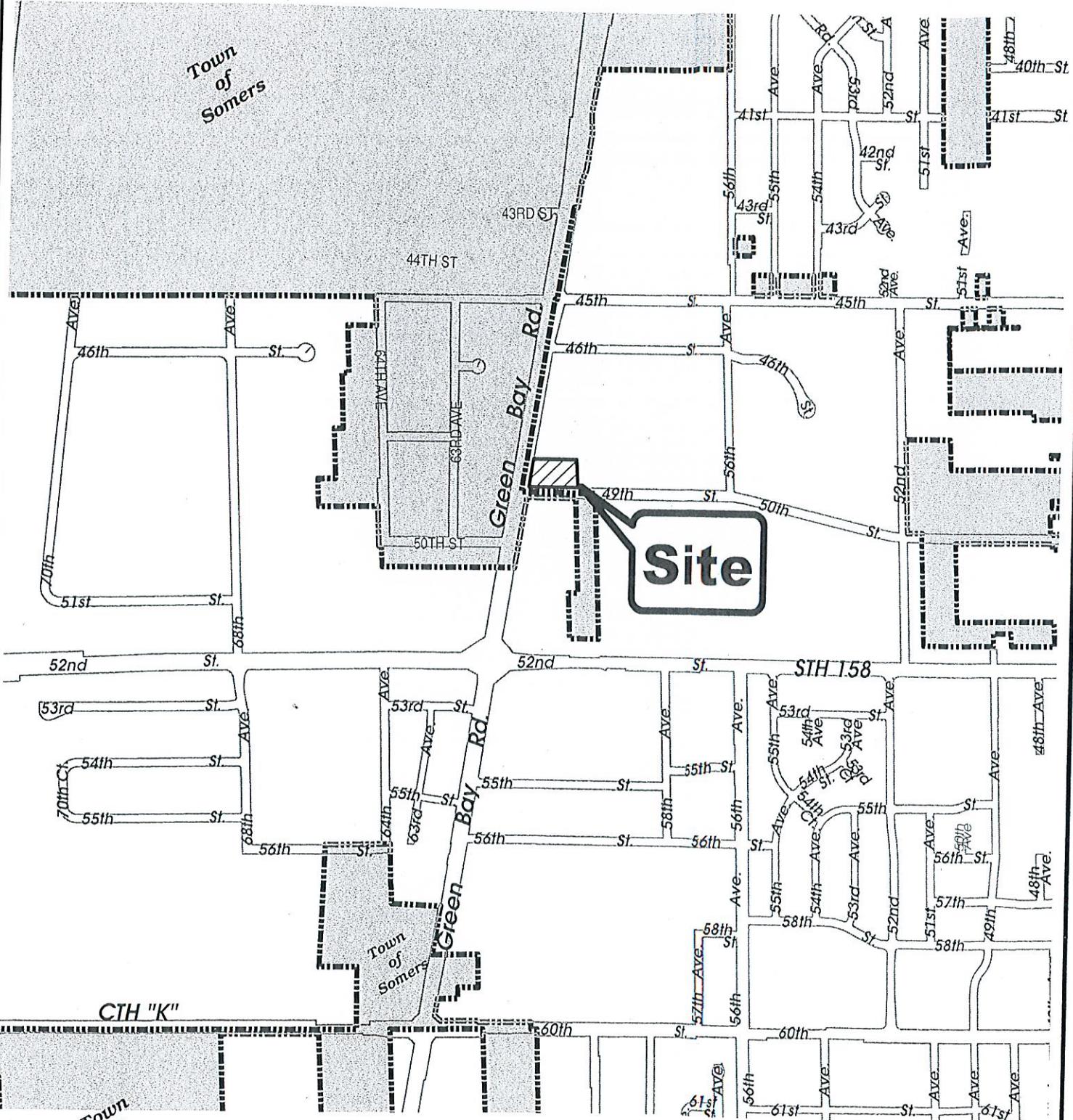
- Provide plans of the facility drawn to 1:20 scale.
- Plan shall show all property lines, existing and proposed structures.

3. Provide parking stall layout and annotate direction of traffic.
4. Show sidewalk on the plan and indicate the dimensions of the driveways on the plan.
5. Provide a lighting plan.
6. Type of permit required will be determined when a complete set of plans is provided.

cc: Jeff Hansen
Clement Abongwa
Kile Kuhlmeier
Gerard Koehler

City of Kenosha

Vicinity Map
Kenosha Recycling Inc. CUP



----- Municipal Boundary



KENOSHA RECYCLING INC Plan of operations

June 20th 2013

Hours of operation

- Monday - Friday 6am to 6pm
- Saturday and Sunday 7am to 2pm

Number of employees

- 3-5

Business activities Primary

- Recycling drop off center of all ferrous and nonferrous metals
- Including but not limited to, Cardboard and paper.

Secondary activities

- Recycling sorting of clean construction debris including but not limited to, wood, plastic, glass, concrete, brick, all non hazardous materials.
- Used building materials such as cabinets and fixtures drop off center for re use sales and donation purposes.

Kenosha Recycling Inc.

Vision: To improve the words environment

Mission: Educate the world HOW to recycle

How:

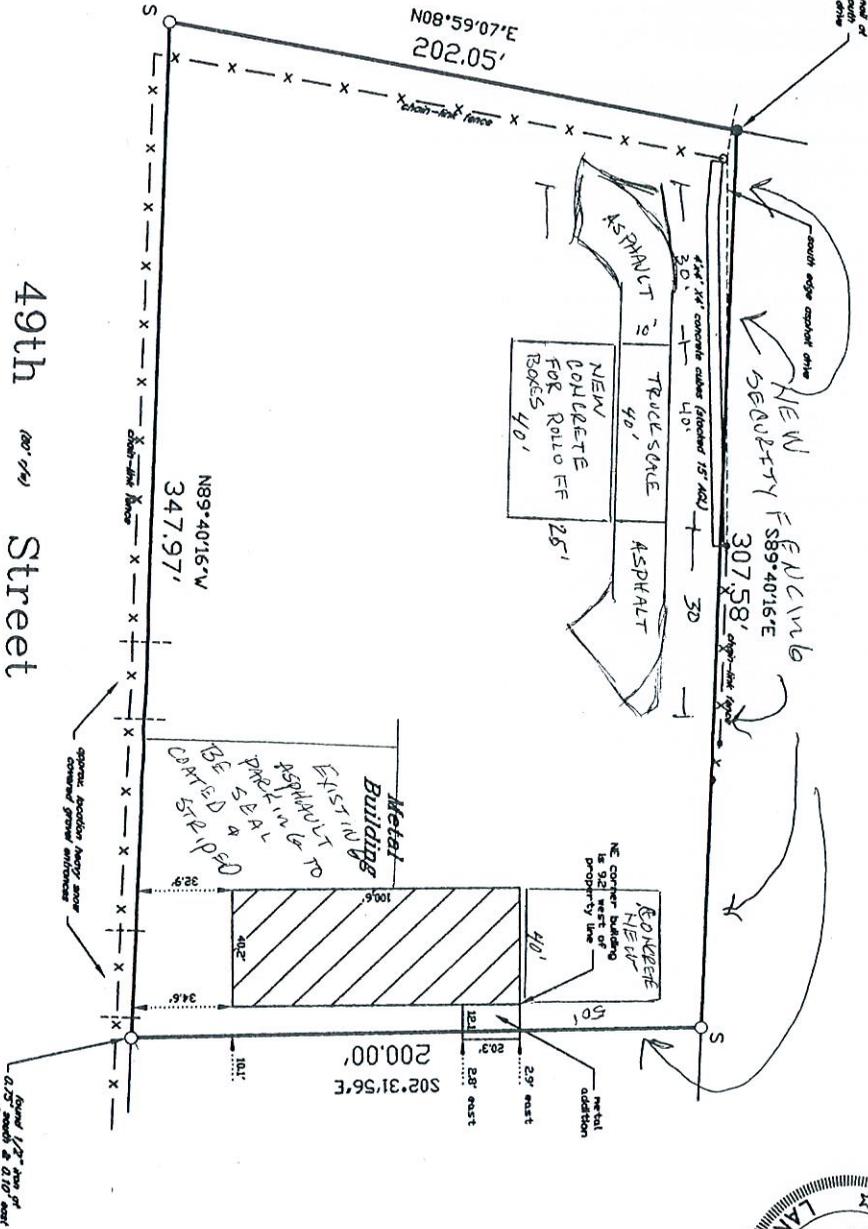
- How to properly recycle
- How to make profits on recycling
- How to improve local community
- How to keep materials out of landfills
- How to live a clean life
- How to be healthy in your recycling efforts
- How to give back to your community at a local level
- How to donate to people in need

08-222-34-102-012

Green Bay Road
St. Tr. Hwy. "31"
(125' / 4')



Scale
1" = 50'



F = found iron pipe
S = set (unless noted)

Refer to a current title report for a complete legal description and for any easements or restrictions which may affect this site.

tax key parcel no.: 08-222-34-102-012

REVISED LOT
PLAN
KENDSHA REYNOLDS



MARESCALCO COUNTYWIDE SURVEYING, INC.
1120 BOUTH STREET KENOSHA, WI 53143
FAX (262) 654 8809

I hereby certify that this property was surveyed under my direction. This plat is a true representation thereof.

Reg. Land Surveyor
January 27, 2005

Bearings refer to grid north, Wisconsin plane coordinate system south zone.

Plat of Survey of
PREMISES LOCATED AT
6000-49TH STREET
in NE1/4 Section 34-2-22
CITY OF KENOSHA
KENOSHA COUNTY, WIS.

-for-
Doug Nelson

40.2

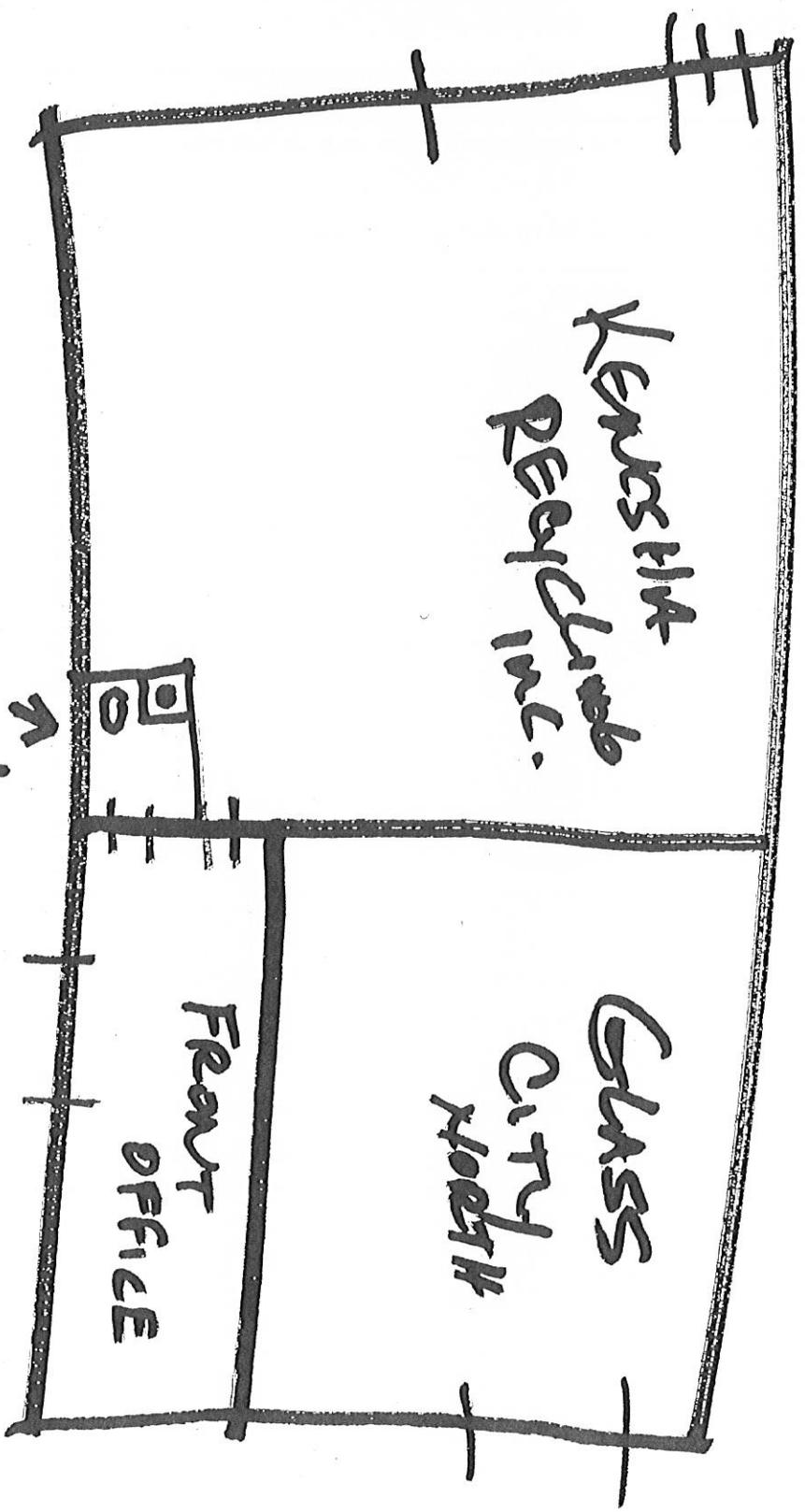
100.6

KENOSHA
RECYCLING
INC.

GLASS
CITY
WOODS

FRONT
OFFICE

BATH
ROOM



08-22-34-102-012

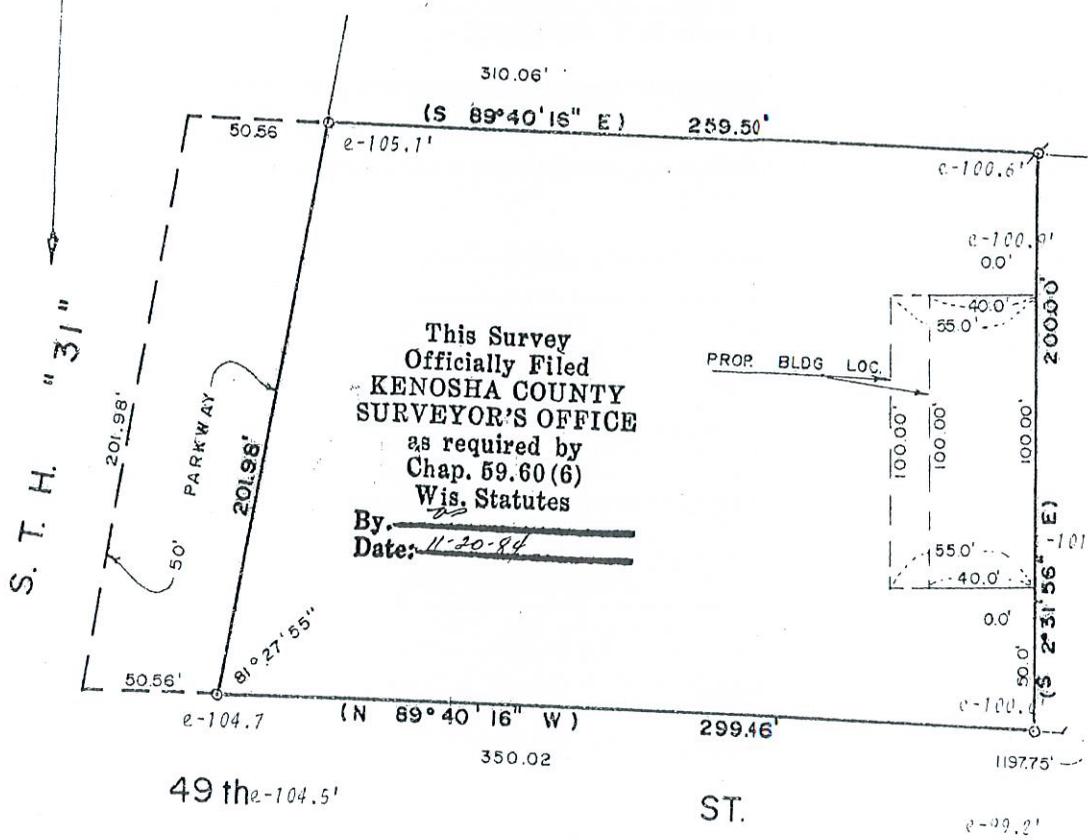


STERNBERG SURVEYING, INC.

Land Surveying, Planning &
Photogrammetric Surveying
815 - 57th Street, Kenosha, WI



NE COR S 34-2-22



This Survey
Officially Filed
KENOSHA COUNTY
SURVEYOR'S OFFICE
as required by
Chap. 59.60 (6)
Wis. Statutes
By: _____
Date: 11-20-84

PLAT OF SURVEY
OF

Part of the NE 1/4 of Section 34, Town 2 North, Range 22 East of the fourth Principal Meridian, lying and being in the City of Kenosha, Kenosha County, Wisconsin and being more particularly described as: Commencing on the east line of said 1/4 Section at a point S2°31'56" 1375.10 feet from the northeast corner thereof; thence N89°40'16"W along and upon the north line of 49th Street and its extension 1197.75 feet and to the point of beginning of the parcel to be here in described; thence continue N89°40'16"W along and upon the north line of said 49th Street 350.02 feet to the east R.O.W. line of S.T.H. 31; thence along and upon said east line of S.T.H. 31 201.98 feet; thence S89°40'16"E parallel to the north line of 49th St 310.06 feet; thence S2°31'56"E parallel to the east line of said 1/4 Section 200.00 feet and to the point of beginning. Containing 1.5 acres more or less.



SCALE 1"=50'

LEGEND

- - IRON PIPE PLACED
- ⊕ - IRON PIPE FOUND
-) - AS PLATTED OR DEEDED
- ⊙ - ELEVATION



I Certify that the above described property has been surveyed under my direction and the above map is a true representation thereof and shows the size and location of the property, its exterior boundaries, the location and dimensions of all visible structures thereon, fences, apparent easements and roadways and visible encroachments, if any.

This survey is made for the exclusive use of present owners of property, and also those who purchase, mortgage or guaranty the title thereto within one (1) year from date hereof.

William M. Penzschin
Registered Land Surveyor

Oct 9, 1984 84180
Date Job No

NE 34-2-22-8



1988



CHESS CITY
AND
MOUTH

**Development Review Application
City of Kenosha, Wisconsin**

MAILING INFORMATION

NAME OF PROJECT: KENOSHA RECYCLING INC.

Check one (1) of the following boxes to indicate the recipient of all correspondence:

- Name and Address of Applicant [Please print]:
KENOSHA RECYCLING INC.
6000 49th ST.
KENOSHA WI 53144
 Phone: 262-857-2626
 Fax: _____
 E-Mail: MIKE@MIKEGOFFMAN
.COM
- Name and Address of Architect/Engineer [Please print]:

 Phone: _____
 Fax: _____
 E-Mail: _____
- Name and Address of Property Owner (if other than applicant)[Please print]:

 Phone: _____
 Fax: _____
 E-Mail: _____

PROJECT LOCATION

Location of Development (street address and / or parcel number): 6000 49th ST. KENOSHA,
WI 53144

TYPE OF LAND DEVELOPMENT

Check all that apply. Note: Additional information may be required within individual Sections.

- | | | |
|---|------------|---------------|
| <input type="checkbox"/> Certified Survey Map | Section 1 | Page 3 |
| <input type="checkbox"/> Concept Review (Land Division) | Section 2 | Page 4 |
| <input type="checkbox"/> Concept Review (Multi-Family Residential or Non-Residential) | Section 3 | Page 5 |
| <input checked="" type="checkbox"/> Conditional Use Permit | Section 4 | Pages 6 & 7 |
| <input type="checkbox"/> Developer's Agreement | Section 5 | Page 8 |
| <input type="checkbox"/> Final Plat | Section 6 | Pages 9 & 10 |
| <input type="checkbox"/> Lot Line Adjustment Survey | Section 7 | Page 11 |
| <input type="checkbox"/> Preliminary Plat | Section 8 | Pages 12 & 13 |
| <input type="checkbox"/> Rezoning | Section 9 | Pages 14 & 15 |
| <input type="checkbox"/> Site Plan Review | Section 10 | Pages 16 & 17 |

**PRIOR TO SUBMITTING THIS APPLICATION TO THE DEPARTMENT OF CITY DEVELOPMENT,
PLEASE REVIEW THE APPROPRIATE SECTION(S) FOR FEES, REQUIREMENTS AND APPROPRIATE APPENDICES.**

*Submit this cover page, completed application, applicable section(s) and appendices
along with ALL required plans, information and fees to:*

Department of Community Development & Inspections
 Planning Division
 625 52nd Street, Room 308
 Kenosha, WI 53140

Phone: 262.653.4030
 Fax: 262.653.4045
 Office Hours:
 M - F 8:00 am - 4:30 pm

**SECTION 4
CONDITIONAL USE PERMIT**

Additional Information Required:

Building or Addition Square Footage: 4000
 Existing Building Size: 4000
 Site Size: 201 x 350 x 200 x 310
 Current # of Employees 3 Anticipated # of New Employees 4-6
 Anticipated Value of Improvements NA

Submittal Requirements:

- > Ten (10) full size scaled copies of Specified Plans indicated below drawn at a standard engineering scale
- > Developer Site Plan/Conditional Use Permit Checklist (Appendix A)

If Item to be Reviewed by Plan Commission/Common Council must Submit:

- > One (1) 8 1/2" x 11" reduction *or* forty (40) 11" x 17" reductions of the Site/Landscape Plan, Floor Plan and Colored Building Elevations (all sides)
- > Sample Board containing colored samples of all exterior building materials

Fees:

	<u>Building or Addition Size</u>	<u>Site size</u>	<u>Review Fee</u>
Level 1	<= 10,000 sq. ft.	<= 1 acre	\$900 = City Plan Dept. <i>or</i> \$1,025 = CPC/CC 50% Fee
Level 2	10,001 - 50,000 sq. ft.	1.01 - 10 acres	\$1,175 = City Plan Dept. <i>or</i> \$1,300 = CPC/CC
Level 3	50,001 - 100,000 sq. ft.	10.01 - 25 acres	\$1,600 = City Plan Dept. <i>or</i> \$1,725 = CPC/CC
Level 4	> 100,001 sq. ft.	> 25.01 acres	\$2,000 = City Plan Dept. <i>or</i> \$2,125 = CPC/CC

\$512.50

- > If building size or addition and gross acreage of the site determine two (2) different fees, the greater of the two fees will be assessed.
- > Application fee entitles applicant to an initial review and one re-submittal.
- > Re-submittal fee = \$425 per re-submittal after two (2) permitted reviews.
- > CUP Amendment = 50% of the applicable fee as determined above.

Appendices to Review:

- > All

Approximate Review Time:

- > 30 days for Staff Review
- > 45-60 days for City Plan Commission/Common Council Review

The conditional use permit plans, prepared to a standard engineering scale, shall be submitted with this application & shall include the following information:

Building Plan:

- > Layout of building(s) including size and layout of rooms
- > Design and architecture
- > Plans and details on fire suppression and/or standpipe
- > Plans and details on fire detection, fire alarm and other safety devices

Site Plan

(based on a plat of survey)

- > Legal description of property
- > Location and footprint of building(s) and structure(s)
- > Locations of existing and proposed streets, drives, alleys, easements, rights-of-way, parking as required, vehicular and pedestrian access points, and sidewalks
- > Outline of any development stages
- > Location and details on any required emergency access roads
- > A calculation of square footage devoted to building, paving and sidewalks, and landscaped/open space

Drainage Plan

EXIST 1/2 b

- > Existing topography, including spot elevations of existing buildings, structures, high points, and wet areas, with any previous flood elevations
- > Floodplain boundaries, if applicable
- > Soil characteristics, where applicable
- > Proposed topography of the site denoting elevations and natural drainage after construction and any proposed stormwater retention areas

ZONING ORDINANCE FOR THE CITY OF KENOSHA, WISCONSIN

center shall be prohibited from processing or flattening aluminum at the site. Automatic aluminum collection machines are exempt from this provision.

p. Other issues which may have an adverse social, economic, or environmental impact or affecting the health, safety or welfare of abutting or neighboring properties or the City as a whole.

9. Recycling Collection Centers.

- a. **Building Plan** as required in §4.05 B. of the Zoning Ordinance.
- b. **Site Plan** as required in §4.05 C. of the Zoning Ordinance.
- c. **Drainage Plan** as required in §4.05 D. of the Zoning Ordinance.
- d. **Landscape Plan** as required by §4.05 E. of the Zoning Ordinance.
- e. **Utility Plan** as required by §4.05 F. of the Zoning Ordinance.
- f. Shall be limited to the collection and processing of Recycling-Center Material, as that term is defined in Chapter 13 of the Code of General Ordinances for the City of Kenosha.
- g. Recycling-Center Material processing shall be limited to those means allowed to a Recycling Center licensee.
 - h. Collection center may be used as an accessory use to an established business.
 - i. A minimum of five (5) parking spaces shall be available for the recycling collection center.
 - j. Recyclable material shall be removed from the recycling collection center at least once a week.
 - k. Owner and/or operator of the recycling collection center shall keep the site clean and in a neat appearance and shall dispose of recyclable material and other litter from the site.
 - l. Sites adjacent to residential zones shall be screened in a manner acceptable to the Administrator.
 - m. Other issues which may have an adverse social, economic, or environmental impact or affecting the health, safety or welfare of abutting or neighboring properties or the City as a whole.

10. Automobile Body Shop in the B-2 and B-3 Districts.

- a. **Building Plan** as required in §4.05 B. of the Zoning Ordinance.
- b. **Site Plan** as required in §4.05 C. of the Zoning Ordinance.
 - (1) Existing buildings and proposed additions or new structures.
 - (2) Customer and employee parking areas.
 - (3) Storage areas for autos and other motor vehicles, parts and trash.
 - (4) Existing and proposed screening and landscaping.
 - (5) Exterior lighting on buildings and poles.
 - (6) Outdoor signs.
- c. **Drainage Plan** as required in §4.05 D. of the Zoning Ordinance.
- d. **Landscape Plan** as required in §4.05 E. of the Zoning Ordinance.
- e. **Utility Plan** as required in §4.05 F. of the Zoning Ordinance.
- f. **Operational Plan** which describes:
 - (1) Hours and days of the proposed operation.
 - (2) Type and extent of work to be done on motor vehicles.
 - (3) How site will be used, especially vehicle storage.
 - (4) How often scrap parts will be picked up.
 - (5) Methods to be used to control noise, paint fumes, and dust.
 - (6) Name and address of body shop operator.
 - (7) Anticipated number of employees.
- g. All repair and painting work shall be conducted wholly within a completely enclosed building.
- h. All storage of vehicles, parts or equipment which is adjacent to or across an alley from any residential district shall be within completely enclosed buildings or contained within accessory outdoor storage areas effectively screened from the residential district, as required in §4.05 E.(2) of the Zoning Ordinance. The Commission may require additional screening or landscaping on any portion of the lot, regardless of the adjacent district, if special characteristics of the body shop warrant such additional screening or landscaping.
 - i. No activity shall produce a sound level outside its premises that exceeds the standards set forth in Chapter 23 "Noise Control" of the City Code of General Ordinances.
 - j. No activity shall emit any odorous matter of such nature or quantity as to be offensive, obnoxious

CODE OF GENERAL ORDINANCES, 2013 - KENOSHA, WISCONSIN

conviction thereof, forfeit not more than Five Hundred (\$500) Dollars, plus the costs of prosecution, and in default of the timely payment thereof be confined in the County Jail for a period not to exceed (30) days.

S. Violations. Each day of a violation of this Ordinance shall be considered a separate offense.

13.011 RECYCLING CENTER ACTIVITY LICENSE

A. Definitions

1. Hazardous Waste: has the meaning of the term used in 40 CFR §261.3 unless excluded in §261.4.
2. Person: person, firm, partnership, association, corporation, company, or organization of any kind.
3. Recycling Center: Place upon which Recycling Center Activity conducted by a Licensee under this section takes place.
4. Recycling Center Activity: The commercial collecting of Recycling-Center Material, storing of Recycling-Center-Material, or engaging in Recycling-Center-Material Processing.
5. Recycling-Center Material: Recycling-Center Metals, batteries, catalytic converters, electronic scrap or stainless steel.
6. Recycling-Center-Material Processing: The crushing of metal items or the reduction of the volume of metal items by shredding, chipping, melting, or any other means.
7. Recycling-Center Metal: Aluminum, copper, tin, and all alloys of aluminum, copper, or tin provided that said alloys do not include gold, silver, platinum, palladium, iridium, rhodium, ruthenium, iron, arsenic, mercury, lead, cadmium, antimony, barium, beryllium, osmium, thallium, vanadium, chromium (IV), or radioactive metals.
8. Recycling-Center Yard: The exterior portions of any building associated with a Recycling Center Activity where Recycling-Center Materials are stored.

B. License Required.

1. Licenses. It shall be unlawful for any person to conduct Recycling Center Activity within the City, whether personally, by agents or employees, singly, or along with some other business or enterprises without first having obtained a license therefor from the Common Council in accordance with the provisions of this ordinance.
2. Other Licenses and Permits. A license issued hereunder shall not exempt the holder thereof from obtaining such other licenses and permits as may otherwise be required by the City or by any other governmental agency.

C. Application. An applicant for a license under this Ordinance shall file with the City Clerk a written application, which is true, correct and complete, signed by himself, if an individual, by all partners if a partnership, and by the president or chief officer of a corporation or other organization, upon forms provided by the City Clerk, together with a fee as hereinafter prescribed. The application shall be sworn to by each of its signers before a notary public or other officer authorized by law to administer oaths and shall include the following information or material:

a. Exact address or location of the place where the business is or is proposed to be carried on, plus a sketch of the actual premises to be used in connection with the business, giving distances in feet and showing fire lanes, property lines, buildings, and abutting roads, particularly identifying the Recycling-Center Yard.

b. A description of the type of construction of any building and structure to be used in connection with the licensed business; a sketch showing the location of such buildings or structures on the business premises, with respect to their distance from roads and fire lanes and a diagram or plan giving distances and heights, showing floors, exits, entrances, windows, ventilators, and walls.

c. A description of any equipment or machinery which will be utilized to process Recycling Center Material and a description of motor vehicles which will be used to collect or haul Recycling Center Material in the operation of the licensed business, including their Vehicle Identification Number, and any D.M.V. or L.C. numbers, where applicable.

d. Such other information as is reasonably necessary to effectuate the purposes of this Ordinance and to arrive at a fair determination of whether the terms of this Ordinance have been complied with.

e. Proof of insurance/financial responsibility for all motor vehicles which will be utilized in the operation of the

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licensed business, in accordance with the requirements of State law.

f. Proof of current registration of all motor vehicles to be used in the course of performing licensed activities.

g. A list of all persons who will operate motor vehicles in the course of performing licensed activities and proof of their possessing a valid and appropriate Wisconsin Driver's License.

D. Application Review. Upon receipt of any application, the City Clerk shall send copies thereof to the Department of Community Development and Inspections, Fire Department, Health Department and Police Department. The Department of Community Development and Inspections, Fire Department and Health Department, either jointly or severally, within ten (10) days of receiving such copies, shall make a report, in writing, as to whether the premises meet the requirements hereof, along with any other pertinent information. The Police Department shall report in writing, to the City Attorney, as to any police record of applicant which may reflect upon their good moral character or business responsibility. The City Attorney shall examine said record and make a recommendation based thereon as to whether or not the license should be granted. Such reports shall be delivered to the City Clerk or clerk for the Committee on Licensing who, in turn, shall deliver them with the application, to the Committee on Licensing. Such Committee shall recommend to the Common Council either the granting or denial of the application or such other action as may be appropriate.

E. Fee. The annual fee for a Recycling Center Activity license is \$350, which shall not be pro-rated.

F. License Form. The Recycling Center Activity license shall bear the following language on its face: "IMPORTANT - This license applies only to the described premises approved by the Common Council and authorizes the Licensee to operate a Recycling Center Activity in a lawful place and manner only; it is not a substitute for any Certificate of Occupancy, Building Permit, Conditional Use Permit, or other licenses, certificates, or permits that might be required by law of the Licensee, and it does not relieve the Licensee of the responsibility of having all such required licenses, permits, or certificates at all times and of complying with all other laws, rules and regulations affecting the business premises."

G. Term/Renewal

1. The term for a Recycling Center Activity licensee is one year, from May 1 or the date of first issue through the ensuing April 30.

2. License renewal applications shall be the same as new license applications, except that Licensee's report of police record need only be updated.

3. Renewal applications must be filed with the City Clerk by March 15th in order to provide for continuous licensing.

H. Non-Transferability. Licenses are not transferable or assignable.

I. Corrections. Applicants and Licensees shall have the duty to amend and correct their application within ten (10) days of such time as any information stated therein is known by Applicant/Licensee to be untrue, incorrect or incomplete. No material change in personnel, scope of operation, or site of operation shall be effective without the advance approval thereof by the Common Council based upon a specific request for such approval.

J. Exemption. The following are exempted from the provisions of this section:

1. Fund raising efforts by not-for-profit organizations;
2. Community clean up efforts by not-for-profit organizations;
3. Community clean up efforts conducted under the auspices of state Adopt-a-Highway program;
4. Licensed scrap salvage dealers or licensed scrap salvage collectors when operating within the operational limitations of the respective licenses; and
5. The City.

K. General Operating Requirements.

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1. Recycling Center Activity licensees. The following general operating requirements shall apply to all Recycling Center Activity licensees licensed in accordance with the provisions of this Section:
 - a. The license issued pursuant to this Section shall be plainly displayed on the business premises.
 - b. The Recycling-Center Yard shall, at all times be maintained in as clean, sanitary and neat of a condition as such premises will reasonably permit.
 - c. An area which is not described in the approved license application shall not be used in the conduct of licensed business.
 - d. No water shall be allowed to stand on the premises in such manner as to, in the opinion of the Health Administrator, constitutes a breeding place for insects or disease that may endanger health.
 - e. Grass and other ground cover on the premises shall be kept at a height of not more than eight (8) inches. The premises shall be substantially free of noxious weeds.
 - f. No garbage, refuse or other waste liable to give off a foul odor or attract vermin shall be kept on the premises, except for domestic garbage which shall be kept in containers which are, in the opinion of the Health Administrator, rodentproof, and removed from the premises as often as is necessary to provide a sanitary environment, but at no less an interval than on a weekly basis.
 - g. No Recycling Center Material which is stored on the premises shall be allowed to rest upon or protrude over any public street, walkway, or curb or become scattered about or blown off the premises.
 - h. Recycling Center Material on the premises, which is not stored in racks or containers, shall be stored in piles not exceeding ten feet (10') in height, at an angle of forty degrees (40°) or less from the ground, and shall be arranged so as to permit access to all such Recycling Center Material for firefighting purposes. Recycling Center Material which is stored in racks or containers shall not be stored at a height in excess of ten feet (10').
 - i. No combustible material of any kind not necessary for the operation of the licensed business, whether within or without a building or structure, shall be kept on the premises, except with the written permission of the Fire Department. The premises shall not be allowed to become a fire hazard.
 - j. Recycling Center Material may not be reduced in volume on the premises through the use of a any apparatus, without Licensee first obtaining a permission therefor from the Common Council which may be granted and issued only upon a showing by Licensee that such activity will be conducted in a manner which is safe and lawful and which will not injure the public health, safety or welfare. Licensee may be requested, as part of the application process, to conduct or arrange for a demonstration, on site if possible, of the acts which are subject to this permit requirement before the Building Inspector or other designee of the Common Council who may make appropriate observations and cause noise, vibration and other tests to be conducted. The Common Council may place reasonable restrictions on the permitted volume-reducing activity, regulating the time, place and manner of operation.
 - k. Licensee may not store or process any elemental metal or alloy of metal on a Recycling Center, unless it is a Recycling-center material.
 - l. Even if volume-reducing apparatus has been otherwise permitted by the Common Council, no process shall be utilized to reduce the volume of Recycling Center Material in a manner that causes vibrations which may damage the property of another or which may be injurious to the public health, safety and welfare. Complaints in this regard shall be processed by the Health Department, which may issue abatement orders, following notice and an opportunity to be heard.
 - m. Licensee shall not directly or indirectly purchase Recycling Center Material from any person that they know, or should know, cannot rightfully or lawfully sell it or deliver clear title thereto.
 - n. Chapter XXIII of the Code of General Ordinances, "Noise Control", shall be strictly complied with.
 - o. The open area of the premises whereupon Recycling Center Material is stored shall be enclosed with a solid, vertical wall or fence of a minimum height of six feet (6') measured from ground level, except where Recycling Center Material is shielded from view by an earthen berm of a minimum height of six feet (6') in which event the required fence need not shield Recycling Center Material from view. Entrances and exits shall not be wider or more numerous than reasonably necessary for the conduct of the licensed business. Fences must be sturdily constructed, safe vertical straight, well maintained and must shield Recycling Center Material from view. Fences must be protected with paint or other preservatives. Fences must not create an appearance of patchwork which is indicative of a state of disrepair. Fences shall be of uniform color which will not detract from the value of abutting and neighboring real estate. No signs may be painted on or attached to any fence, except for a sign of not more than eight square feet (8 ft²), located at an entranceway thereto which provides the name of the business. Fences must

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be constructed in such a fashion and of such materials and color as are approved by the Building Inspector prior to fence construction, repair or replacement. Chain link fencing shall be appropriate where opaque strips are inserted in the fence. No fence may be constructed or replaced without the approval of the Building Inspector, or designee thereof, which approval may be denied if the above standards have not been complied with or if the construction of a proposed new fence or replacement fence would violate any other provision of law. No Recycling Center Material may be stored outside of said fence or may be stored so as to rest against said fence, protrude through it, or hang over it. Fence gates shall be kept closed when not required to be open for ingress or egress, except where a buffer fence is constructed in the vicinity of fence gates within the Recycling-Center Yard which shields Recycling Center Material from view when the gates are open. In all other respects, fences shall be installed and constructed in accordance with City Ordinances.

p. No Recycling Center Activity licensee or his or her agent or employee shall, except for aluminum cans, purchase or receive any Recycling Center Material for use in the licensed business from any person under the age of eighteen (18) years without the written consent of a parent or guardian of such person. Such writing shall be held available for inspection by any member, or representative, of the Police Department for one (1) year.

q. Each acquisition of such Recycling Center Material as is capable of being used for its original and intended purpose without repair or alteration, including all motor vehicle parts, except for bulk purchases from industrial and commercial concerns, shall be recorded in English in a permanent type register kept on the business premises, giving the name and residence address of the person from whom the acquisition was made, a description of the Recycling Center Material acquired, any identification or serial number, and the date of the transaction. Such data shall be held available for inspection by any member, or representative, of the Police Department for one (1) year. The identification of each seller of a motor vehicle or motor vehicle part shall be checked and the type of identification checked noted, except where the purchase is from a licensed motor vehicle dealer, an insurance company, a City licensed tower or an auction.

r. No Recycling-Center Yard shall be allowed to become a public or attractive nuisance; nor shall any Recycling-Center Yard be operated in such manner as to adversely affect the public health, safety, or welfare.

s. There shall be full compliance with the City Building, Fire, and Health Codes and with all other City, County, State and Federal laws, rules or regulations which may be applicable.

t. The Fire Department may order Licensees to install fire lanes, following notice and opportunity to be heard, and upon providing a reasonable time for compliance.

u. The terms of a Conditional Use Permit which are not inconsistent with this Ordinance, shall be complied with at all times as a condition of license maintenance.

v. No Recycling Center Material shall be collected which is placed on a City right-of-way for City collection and no Recycling Center Material shall be deposited or sorted on a City right-of-way or removed from the property of any nonconsenting person or party.

w. The licensed premises must possess appropriate zoning for the licensed business or constitute a lawful nonconforming use.

x. Motor vehicles which are used in the licensed business shall hold and display an L.C. and M.V.D. number where such number is required by State law, rule or regulation, shall be insured or financially responsible in accordance with State law, shall be properly and currently registered with the State Motor Vehicle Department, and shall have the licensee's name conspicuously and legibly printed thereon.

y. Hazardous Waste may not be collected, stored, or processed on the business premises.

L. Licensees/Responsibility. Recycling Center Activity licensees shall, with respect to the conduct of their business, have a duty to supervise their agents and employees and shall be responsible for the acts thereof.

M. Suspected Stolen and Evidentiary Recycling Center Material. The Police Department may order any licensee hereunder to retain any Recycling Center Material upon the premises which is suspected to be stolen for a period not to exceed thirty (30) days pending an investigation. The Police Department, at any time, may seize from the premises, as evidence, any Recycling Center Material which they have probable cause to believe is stolen, and may hold said Recycling Center Material for so long as it shall have evidentiary value. Any seized item shall be released within a reasonable time upon prosecution being denied by any prosecuting attorney. Any enforcing department may order a Licensee to retain Recycling Center Material upon the premises which is suspected of being held in violation of this Ordinance for a period not to exceed thirty (30) days during which an investigation can be conducted. Any enforcing department may take samples of Recycling Center Material for

CODE OF GENERAL ORDINANCES, 2013 - KENOSHA, WISCONSIN

purposes of testing or arrange for any other governmental agency to do so.

N. Conditional Use Permits. Conditional Use Permits, as required by the Zoning Ordinance, shall be required prior to any unlicensed premises being licensed and prior to the expansion of any existing licensed premises, and each license shall be conditioned upon obtaining and complying with the provisions of a Conditional Use Permit, where required. A Conditional Use Permit may contain requirements over and above the requirements of this Ordinance, but may not negate any of the requirements of this Ordinance, and if it would do so, any conflicting provisions thereof shall be null and void.

O. Appeals From Orders. A Licensee, where the public health, safety and welfare is not in immediate jeopardy, may appeal to the Common Council any order issued by any enforcing Department under authority of this Ordinance by filing a written Notice of Appeal with said Department within ten (10) days of receipt of said order, or within the compliance period, whichever is shorter. The Common Council may direct that the appeal be heard by a Committee thereof.

P. Revocation and Suspension Of Licenses. The Common Council may, for just cause, suspend, revoke or not renew any license herein provided, upon serving such party written notice of the charges forming a basis for the proposed penalty, in the same manner as that for the service of a Summons in a civil action. Just cause shall include, but not be limited to:

1. Failing to maintain a status of good moral character and business responsibility.
2. Obtaining the license through fraud or misrepresentation.
3. Operating contrary to the terms of this Ordinance.
4. Failing to commence doing business within ninety (90) days of being granted a license or within ninety (90) days of the first day of the license term, whichever is longer, unless an extension of time is applied for and granted by the Committee on Licenses and Permits.
5. Suspending doing business for a period of ninety (90) consecutive days, unless permission is applied for and granted by the Committee on Licenses and Permits.

Q. Disciplinary Hearings. Disciplinary hearings, including nonrenewal, suspension and revocation hearings, may be held before the Common Council or before the Committee charged with license review responsibilities.

The Committee on Licenses and Permits, when it conducts a hearing, shall submit a report to the City Common Council, including findings of fact, conclusions of law and a recommendation as to what action, if any, the City Common Council should take with respect to the license. Said Committee shall provide the complainant and the licensee with a copy of the report. Either the complainant or Licensee may file an objection to the report and shall have the opportunity to present arguments supporting the objection to the Common Council. The City Common Council shall determine whether the arguments shall be presented orally or in writing, or both. If the City Common Council, after considering the Committee's report and any arguments presented by complainant or Licensee, finds the complaint to be true, or if there is no objection to a report recommending a suspension, revocation or nonrenewal of the license, it shall be suspended, revoked or not renewed as provided by law. If the City Common Council finds the complaint untrue, the proceedings shall be dismissed without cost to the accused. The City Clerk shall give notice of each suspension, revocation or nonrenewal to the party whose license is affected. The Common Council may also order corrective action to be taken within a specified time as a condition of license maintenance, at any time, following notice and an opportunity to be heard.

R. Penalty. Any person who shall violate any of the terms and conditions of this Ordinance shall, upon conviction thereof, forfeit not more than Five Hundred (\$500) Dollars, plus the costs of prosecution, and in default of the timely payment thereof be confined in the County Jail for a period not to exceed (30) days.

S. Violations. Each day of a violation of this Ordinance shall be considered a separate offense.

13.0112 NON-STRUCTURAL DEMOLITION

Planning & Zoning Division 625 52nd Street - Room 308 Kenosha, WI 53140 262.653.4030	Kenosha City Plan Commission FACT SHEET	August 8, 2013	Item 2
Conditional Use Permit for a 105,387 s.f. addition to the Carthage Straz Center at 2001 Alford Park Drive. (Carthage College) (District #1) PUBLIC HEARING			

LOCATION/SURROUNDINGS:

Site: 2001 Alford Park Drive
 Zoned: IP Institutional Park

NOTIFICATIONS/PROCEDURES:

The alderman of the district, Alderman Haugaard, has been notified. The Common Council is the final review authority.

ANALYSIS:

- Carthage College is proposing to renovate and add onto the existing Straz Center on their campus. The existing building houses classrooms and office spaces. The addition will include more classrooms as well as common areas for students and faculty.
- The primary building exterior materials are Lannon stone, brick and glass.
- The project will include extensive landscaping and outdoor seating areas, along with a proposed rain garden.
- Carthage will need to obtain a Special Exception from the Common Council for the additional classroom space. Overall, Carthage will be providing more parking than required, but several of the spaces are located off-site. Since the spaces are not within 800 ft. of campus, the Special Exception for parking is required.
- Plans were sent to City Departments for their review. Their comments are included in the attached Conditions of Approval.
- The plans generally comply with Sections 4 and 14 of the Zoning Ordinance.

RECOMMENDATION:

A recommendation is made to approve the Conditional Use Permit, subject to the attached Conditions.



Brian R. Wilke, Development Coordinator



Jeffrey B. Labahn, Director

Planning & Zoning Division 625 52nd Street Kenosha, WI 53140 262.653.4030	<i>Kenosha City Plan Commission Conditions of Approval</i>	Carthage Straz Center 2001 Alford Park Drive	August 8, 2013
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1. The following Conditions of Approval will run with the land and shall be included in a document recorded with the Kenosha County Register of Deeds:
 - a. The applicant shall obtain all required construction permits from the Department of Community Development & Inspections. This includes, but is not limited to Erosion Control, Building, Plumbing, Electrical and Occupancy permits.
 - b. All signs shall comply with Chapter 15 of the Code of General Ordinances and the applicant shall obtain sign permits for all new signs. A separate submission and permits are required for all signs. Sign information provided under the Conditional Use Permit is for reference only and does not constitute sign permit approval.
 - c. The development shall be constructed per the approved plans on file with the Department of Community Development & Inspections, Room 308, 625 52nd Street, Kenosha, WI 53140. Any changes to the approved plans shall require an amendment to the Conditional Use Permit. All changes shall be submitted to the Department of Community Development & Inspections for review and approval.
 - d. Prior to the issuance of any Occupancy permits, all parking areas, drives and designated paved areas shall have the initial lift of asphalt installed. The building exterior shall be completed per the approved plans, the exterior lighting shall be installed and the Conditional Use Permit shall be recorded with the Kenosha County Register of Deeds. All improvements indicated on the plans, including landscaping, shall be installed prior to the issuance of a final Occupancy permit. The recording fees for the Conditional Use Permit shall be submitted by the applicant.
 - e. Compliance with City and State and/or Federal Codes and Ordinances. The buildings shall comply with the current Code standards in effect upon application for a building permit.
 - f. All roof top mechanicals shall be properly screened per Section 14.07 B.10 of the Zoning Ordinance.
 - g. All trash containers shall be stored within the enclosure or building. The applicant/owner shall be responsible for waste collection and removal for the development. The City of Kenosha shall not provide waste collection or removal services or incur any cost in this regard.
 - h. The applicant shall meet all applicable Conditions of Approval and obtain a building permit within six (6) months of Common Council approval of the Conditional Use Permit or the Conditional Use Permit shall be null and void.
 - i. All vehicles shall be parked within the designated paved areas.

Planning & Zoning Division 625 52nd Street Kenosha, WI 53140 262.653.4030	<i>Kenosha City Plan Commission Conditions of Approval</i>	Carthage Straz Center 2001 Alford Park Drive	August 8, 2013
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- j. All improvements, including landscaping, shall be maintained per the approved plans. Any damaged fencing, landscaping or building shall be replaced or reconstructed per the approved plans.
 - k. Cross access shall be provided to adjacent parcels if required at a future date by the City.
2. The following conditions of approval shall be satisfied with City Staff prior to the issuance of any construction permits.
- a. The Drainage Plan shall be revised and resubmitted for review and approval addressing comments listed in the Public Works memo dated July 25, 2013.
 - b. The Utility Plan shall be revised and resubmitted for review and approval addressing the comments listed in the Kenosha Water Utility Memo dated July 22, 2013.
 - c. A Special Exception shall be obtained from the Common Council for the number of off-street parking spaces.

/u2/acct/cp/ckays/1CPC/2013/AUG8/conditions-carthage.odt

Engineering Services

4401 Green Bay Road
Kenosha WI 53144

Phone (262) 653-4315
Fax (262) 653-4303



"Providing and Protecting Kenosha's Greatest Natural Resource"

MEMO

To: Brian Wilke, Development Coordinator

From: Ian C. Bagley, P.E., Water Engineer

Date: July 22, 2013

Subject: Carthage College Straz Center Addition and Renovation

Location: 2001 Alford Park Drive

The Kenosha Water Utility (KWU) has reviewed the submittal for the above referenced project along with email correspondence on July 16th and July 19th, 2013. Further information from the Developer is required before the Utility can complete this review. Please withhold all permits until the following information is submitted.

1. The construction specifications shall be revised and resubmitted according to the comments given to John Bakaysa, via email, on July 18th 2013.
2. The water meter shall have unobstructed access, twelve inches (12") from the inside wall, twelve to twenty-four inches (12"-24") above the floor. The meter shall have a gate valve on the inlet and outlet pipe.
3. All water meters one and one half inches (1-1/2") or greater shall have a bypass with a two way ball valve with locking handle as manufactured by RuB, or approved equal.
4. Meters three inches (3") and larger shall have a two inch (2") test plug provided between the outlet side of the meter and the outlet valve.
5. A 3C18 gauge cable by Belden-M, or approved equal, shall be installed in a one half inch (1/2") conduit through the exterior wall for the remote meter reader. Remote reader to be field located by KWU meter division.
6. The KWU water main and sanitary sewer detail sheets shall be included on a separate sheet and shall not be placed within the designer's title block or include utility plan notes specific to this project. These detail sheets can be found on the KWU website (<http://www.kenoshawater.org/engineering.html>).

CC: Mr. Robert Carlson, P.E., Director of Engineering Services



ENGINEERING DIVISION
SHELLY BILLINGSLEY, P.E.
CITY ENGINEER

PARK DIVISION
JEFF WARNOCK
SUPERINTENDENT

FLEET MAINTENANCE
MAURO LENCI
SUPERINTENDENT

STREET DIVISION
JOHN H. PRIJIC
SUPERINTENDENT

WASTE DIVISION
ROCKY BEDNAR
SUPERINTENDENT

DEPARTMENT OF PUBLIC WORKS
MICHAEL M. LEMENS, P.E., DIRECTOR
SHELLY BILLINGSLEY, P.E., DEPUTY DIRECTOR

MUNICIPAL BUILDING · 625 - 52ND ST · RM 305 · KENOSHA, WI 53140
TELEPHONE (262) 653-4050 · FAX (262) 653-4056
EMAIL PUBLICWORKS@KENOSHA.ORG

TO: Brian Wilke, Development Coordinator

FROM: Michael M. Lemens, P.E.
Director of Public Works

Shelly Billingsley, P.E.
City Engineer

DATE: July 25, 2013
SUBJECT: PLAN REVIEW COMMENTS
Project Description: Carthage College Straz Center Modernization and Expansion
Location: 2001 Alford Park Drive

Our staff has reviewed the plans for this project. The following comments are provided:

Parking Lot Ordinance Compliance	Sufficient	Deficient	Not Applicable
Parking Lot Paved			X
Standard Stall Width			X
Parking Lot Layout			X
Parking Lot Lighting Shown			X
Parking Lot Lighting Adequate			X
Handicapped Parking			X
Driveway Locations			X
Driveway Width			X
Passing Blister or Accel/Decel Lanes			X
Sidewalks Adequate			X
Drive Thru Lane Design			X

Public Streets	Sufficient	Deficient	Not Applicable
Geometric Design			X
Pavement Width			X
Pavement Thickness Design			X
Established Grades			X
Plan Details			X
Sidewalks			X
Street Lights			X

Site Grading/Drainage	Sufficient	Deficient	Not Applicable
Drainage Plan		X	
Storm Sewer	X		
Storm Water Detention		X	
Drainage Calculations		X	

Project Approval/Permits Needed	Yes	No	Not Applicable
Project Approved for Permitting		X	
Withhold Permits: See Comments	X		
Approve Footing/ Foundation Only (per condition)			X
Parking Lot Permit Required			X
Driveway Permits Required			X
Sidewalk Permit Required			X
Street Opening Permit Required			X
Stormwater Permit Required	X		
Erosion Control Required	X		
State Permit Required	X		

Grading & Drainage Comments:

1. Note that the erosion control review is separate from the CUP review and the erosion control plan will be reviewed when the erosion control permit is applied for.
2. Plans and the stormwater management plan (SWMP) must be stamped by a professional engineer registered in Wisconsin.
3. Put the datum of all benchmarks on the plans.
4. Show on the site plan a table showing the total disturbed area of the project and the pre and post construction square feet of impervious surfaces within the disturbed area.
5. Show additional spot elevations at the curb so that an elevation is shown a minimum of every 50' along curb lines and include all high and low points.

6. Provide construction details for storm sewer structures.
7. The utility plan contains a note that states "See C8.01 for rain garden detail". Sheet C8.01 was not included in the submitted plan set.
8. The utility plan appears to show an existing storm sewer connection to the building just north of "EX Storm MH 1". The plans does not call out for abandoning any storm connection at this point but does call out for a new sanitary connection at this location (labeled San Connect 2). Clarify if an existing sanitary discharge is connected to the existing storm sewer. If the building sanitary is currently discharging through the storm sewer this must be disconnected immediately.
9. The submitted Stormwater Management Plan Worksheet lists "NA" for all items in the "Peak Discharge" category. The site is required to meet peak discharge requirements as stated in the City's published stormwater criteria. The applicable section of the criteria reads as follows:

4. Applicability for Control of Peak Runoff Rates: (contact the Kenosha Stormwater Utility to determine if the development is in an area covered by a regional stormwater management plan and what the requirements of this plan are) For development in areas not covered by a regional stormwater management plan; control of peak runoff for the 2 year 24 hour storm shall be required for all new development, in-fill development, composite development, or redevelopment consisting of 1 acre or more to maintain the post development runoff for this design storm to no more than the predevelopment level. Control of the 10 through 100 year 24 hour storms is also required in any area where there is inadequate storm sewer or drainage-way capacity.

10. The SWMP needs to contain a written narrative describing the pre and post construction site and contain all of the requirements from section 36.09 of the City's stormwater ordinance. Submitting a copy of the worksheet is not sufficient.
11. In the SWMP provide a table summarizing the SLAMM modeling. The table should include the amount of TSS load generate by the entire disturbed area with no controls, the load captured and removed by each proposed BMP, and a calculation of the total percentage of TSS removed for the site.
12. The SLAMM model indicates a bottom infiltration rate of 0.5 inches per hour for the rain garden. Provide documentation supporting this infiltration rate.
13. Provide an exhibit in the SWMP delineating the drainage area to each catch basin and showing what areas within the project limits drain either directly to the rain garden or are uncaptured by the proposed project.

Traffic Comments:

No comments

cc: Jeff Hansen
Clement Abongwa
Kile Kuhlmeier
Gerard Koehler

Zimbra

rschroeder@kenosha.org

Re: Carthage Straz Center Addition

From : William Hoare <whoare@carthage.edu> Tue, Jul 30, 2013 11:14 AM
Subject : Re: Carthage Straz Center Addition 📎 2 attachments
To : Brian Wilke <bwilke@kenosha.org>
Cc : Raymond Maggi <Raymond.Maggi@burthill.com>, John Bakaysa <John.Bakaysa@stantec.com>, Rich Schroeder <rschroeder@kenosha.org>, Jeff Labahn <JLabahn@kenosha.org>

Dear Brian,

Per your request, attached please find two updated documents. One is a current inventory of campus buildings and the square footage of spaces as defined in the Parking Requirement Ordinance of the City of Kenosha. On the lower section of the page you will find the College's current inventory of parking spaces as defined by that same Ordinance. I have included only those buildings and parking spaces within the City of Kenosha. As a quick summary, the Ordinance requires Carthage to own a portfolio of 1315 parking spaces while we currently maintain an inventory of 1870 spaces. This inventory includes the parking lot located at the former Pepsi site (35th Street), our Lot P, and the new lot on 14th Avenue, our Lot R. The 1315 parking space requirement takes into consideration that the Campbell Student Union was granted an exemption thus alleviating the requirement for 32 spaces as designated by that buildings square footage.

The second document forecasts the change in our facilities and parking requirements due at the completion of the Science Center Project. That project adds 35,000 square feet of new campus floorspace while remodeling 70,000 square feet of existing lab/classroom building. The net change increases the parking requirement obligation of the College by 64 parking spaces. The project also consumes the land now occupied by our current parking lot E thus reducing our parking inventory by 49 spaces.

Even with the increase in our parking space obligation, Carthage still maintains an excess inventory of parking resources. This is primarily due to the increase of 517 parking spaces in August of 2011 by the construction of our 14th Avenue Lot R located at the former MacWhitney industrial site. This project, completed with the cooperation and partnership of the City, has not only solved the day to day practical parking needs of the College, but promises to continue to provide these benefits for years to come.

I will be happy to answer any questions you may have regarding this inventory and projection. I thank you for your assistance in this matter and your continued service to Carthage.

Sincerely,

Bill Hoare

William D. Hoare
Associate Vice President for Business
Carthage College
(262) 551 5730 office
(262) 552 5495 fax
whoare@carthage.edu

Forecasted September 2015 Post Construction of New Science Center

Carthage College
Compliance Worksheet - City of Kenosha Parking Ordinance
August 2013 Updated

Building Name	Gross Sq. Ft.	Net Square Footage per Ordinance Designated Purposes							Assembly Seating Capacity		Required Parking Spaces	
		Classroom	Conference Rm.	Office	Museum	Library	Art Gallery	Sq. Ft. Usage	Assembly	Total	Exempt	Exempt
623 17th Place Home	2400	0	0	0	0	0	0	0	0	0	0	0
626 17th Place Home	1500	0	0	0	0	0	0	0	0	0	0	0
David A. Straz Center (NEW SCIENCE CENTER)	155000	32624	8460	400	0	0	0	0	164	57	221	0
Denhart Resident Hall	52942	0	0	0	0	0	0	0	0	0	0	0
Fritsch Chapel	400	0	0	0	0	0	0	0	0	0	0	0
H F Johnson Center	52267	6542	0	4282	0	0	2600	0	54	6	6	0
Hedberg Library	65000	2261	0	10180	0	28149	0	0	162	0	162	0
Joseph Johnson Residence Hall	51960	0	0	520	0	0	0	0	2	0	2	0
Lentz Hall	111900	19423	1500	19332	0	0	0	0	161	0	161	0
Oaks Building #1	27800	0	280	0	0	0	0	0	1	0	1	0
Oaks Building #2	27800	0	280	0	0	0	0	0	1	0	1	0
Oaks Building #3	27800	0	280	0	0	0	0	0	1	0	1	0
Oaks Building #4	27800	0	280	0	0	0	0	0	1	0	1	0
Oaks Building #5	27800	0	280	0	0	0	0	0	1	0	1	0
Oaks Building #6	27800	0	280	0	0	0	0	0	1	0	1	0
Potentia Chapel	440	0	0	0	0	0	0	0	0	0	0	0
Selbert Chapel	35997	0	0	1795	0	0	0	0	0	0	0	0
Campbell Student Union	34200	0	151	631	0	0	0	0	7	214	221	0
Smeds Tennis Center	2500	0	0	200	0	0	0	0	0	0	0	0
Madrigano Family Residence Hall	57512	0	0	11502	0	0	0	0	46	0	46	0
Swenson Residence Hall	5000	0	0	0	0	0	0	0	0	0	0	0
Tarble Arena	65300	6535	0	872	0	0	0	0	30	350	380	0
Tarble Athletic and Recreation Center	155600	1404	800	8046	0	0	0	0	41	28	69	0
Tarble Residence Hall	44270	0	0	0	0	0	0	0	0	0	0	0
Todd Wehr Center	62729	0	9498	1611	0	0	0	0	44	0	44	0
Totals	1123617	68789	136229	67431	0	28149	2600	0	719	660	1379	4820

Carthage College Parking Lot Survey
Updated August 2013

Lot	Designated	Student Spaces	Fac/Staff Spaces	Handicapped Spaces	Other Reserved	Total Lot
H	Com F/S	141	4	4	14	145
C	Com F/S	150	3	3	153	153
Straz Center North Lot N	Fac/Staff	28	2	2	30	30
Straz Center East Lot E	Fac/Staff	0	0	0	0	0
LH Garage & Maint	Fac/Staff	26	1	1	27	27
D	Commuter	30	0	0	30	30
Lentz East "Visitors"	Restricted	2	4	4	14	20
JFAC South	Fac/Staff	5	4	4	1	10
I	Restricted	59	4	4	17	80
A	Restricted	26	10	10	9	557
J	Restricted	0	6	2	2	17
P (35th Street)	Restricted	501	5	5	125	125
V	Restricted	120	113	2	9	124
R (14th Street)	Restricted	517	6	6	517	517
Campus Houses in Kenosha	Restricted				6	6

Student Spaces	Fac/Staff Spaces	Handicapped Spaces	Other Reserved	Total Lot
1572	152	40	57	1821

TOTALS GROSS PARKING INVENTORY

Carthage College
 Compliance Worksheet - City of Kenosha Parking Ordinance
 March 2013 Updated
 Current (August 2013) Prior to Construction of New Science Center
 By: WDH

Building Name	Gross Sq. Ft.		Net Square Footage Per Ordinance Designated Purposes							Assembly Seating Capacity			Required Parking Spaces		
	Classroom	Conference Rm.	Office	Museum	Library	Art Gallery	Seating Capacity	Sq. Ft Usage	Assembly	Total	Exempt	Exempt	Total		
623 17th Place Home	2400	0	0	0	0	0	0	0	0	0	0	0	0		
626 17th Place Home	1500	0	0	0	0	0	0	0	0	0	0	0	0		
David A Straz Center	120000	19389	0	5701	0	0	400	100	57	157	0	0	0		
Denhart Resident Hall	52942	0	0	0	0	0	0	0	0	0	0	0	0		
Fritsch Chapel	400	0	0	0	0	0	40	0	0	6	6	6	6		
H F Johnson Center	52267	6542	0	4282	0	0	2600	54	0	54	0	0	54		
Hedberg Library	65000	2261	0	10180	0	0	28149	162	0	162	0	0	162		
Joseph Johnson Residence Hall	51960	0	0	520	0	0	0	161	0	161	0	0	161		
Lantz Hall	111900	19423	1500	19332	0	0	0	1	0	1	0	0	1		
Oaks Building #1	27800	0	280	0	0	0	0	1	0	1	0	0	1		
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Oaks Building #3	27800	0	280	0	0	0	0	1	0	1	0	0	1		
Oaks Building #4	27800	0	280	0	0	0	0	1	0	1	0	0	1		
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Oaks Building #6	27800	0	280	0	0	0	0	1	0	1	0	0	1		
Potent Chapel	440	0	0	0	0	0	32	0	0	32	0	0	32		
Seibert Chapel	35897	0	0	1795	0	0	0	7	214	221	0	0	221		
Campbell Student Union	34200	0	151	631	0	0	0	Exempt	Exempt	Exempt	1	0	1		
Smeets Tennis Center	2500	0	0	200	0	0	0	46	0	46	0	0	46		
Madrigano Family Residence Hall	57512	0	0	11502	0	0	0	0	0	0	0	0	0		
Swenson Residence Hall	5000	0	0	0	0	0	0	0	0	0	0	0	0		
Tarble Arena	65300	6535	0	872	0	0	0	30	350	380	0	0	380		
Tarble Athletic and Recreation Center	155600	1404	800	8046	0	0	198	41	28	69	0	0	69		
Tarble Residence Hall	44270	0	0	0	0	0	0	0	0	0	0	0	0		
Todd Wehr Center	62729	0	9498	1611	0	0	0	44	0	44	0	0	44		
Totals	1088617	55554	13629	64672	0	28149	2600	655	660	1315	4820	660	1315		

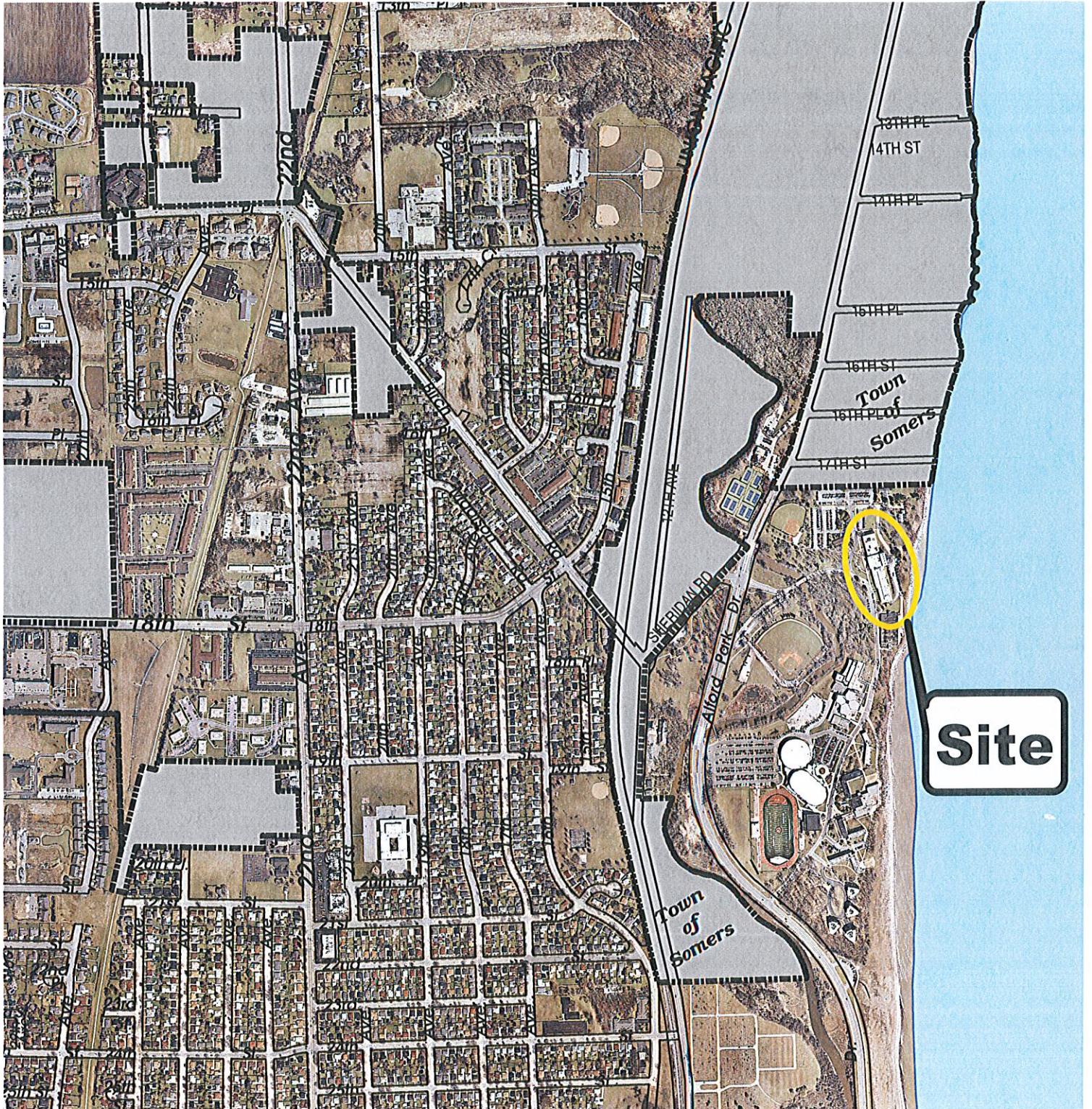
Carthage College Parking Space Inventory
 Updated March 2013

Lot	Designated	Student Spaces	Fac/Staff Spaces	Handicapped Spaces	Other Reserved	Total Lot
H	Com F/S	141	0	4	0	145
C	Com F/S	150	0	3	0	153
Straz Center North Lot N	Fac/Staff	0	28	2	0	30
Straz Center East Lot E	Fac/Staff	0	47	2	0	49
LH Garage & Maint.	Fac/Staff	0	26	0	1	27
D	Commuter	30	0	0	0	30
Lantz East "Visitors"	Restricted	0	2	4	14	20
JFAC South	Fac/Staff	0	5	4	1	10
I	Restricted	59	4	4	17	80
A	Restricted	501	26	10	9	537
J	Restricted	0	6	2	9	17
P (35th Street)	Restricted	120	0	5	0	125
V	Restricted	113	0	2	9	124
R (14th Street)	Restricted	517	0	0	6	517
Campus Houses in Kenosha	Restricted	0	0	0	0	6
TOTALS GROSS PARKING INVENTORY		1572	199	42	57	1870

Parking Spaces Required in Fall 2012 per City Ordinance

City of Kenosha

Vicinity Map
Carthage College
Straz Center Addition CUP



Site



----- Municipal Boundary

Executive Summary

The Carthage College Straz Center Modernization and Expansion Project will endeavor to transform the existing David A. Straz, Jr. Center for the Natural and Social Sciences into a contemporary, state-of-the-art science facility. The project includes the renovation and expansion of the existing facility to incorporate modern teaching labs, new faculty office spaces, and interactive learning zones. The new design will create and present a new identity for the sciences along Campus Drive. The expansion will take advantage of its lakeside setting to afford ample views and interaction with Lake Michigan.

The building program for the Straz Center Modernization and Expansion Project is divided into the following main categories: General Classrooms, Teaching Laboratories, Research Laboratories, Laboratory Support Spaces, Lounges, Offices, Building Support, and Theater. The theater component of the program summary is a series of existing spaces on the Lower Level of the existing facility that are part of the Performing Arts Department; however, they are located within the limits of the project and shall only be modified to meet include new MEP/FP upgrades.

The existing David A. Straz, Jr. Center for the Natural and Social Sciences is part of a larger existing building that includes both the Claussen Center for World Business and the Main Auditorium on campus. The existing Straz Center comprises three floors on the south side of the building and accounts for approx. 60% of the entire facility. The Expansion and Renovation project will include all three levels of the existing Straz Center for a total of 70,298 Gross SqFt. The addition will create another 73,881 Gross SqFt. for a total project size of 103,433 Gross Sqft. All renovation and construction activities will have to be phased to keep the building fully operational. The campus does not have another science facility and teaching lab swing space is not available.

The Straz Center Modernization and Expansion Project will need to consider the existing building in its entirety from a life safety perspective. Exiting and egress strategies will need to provide properly sized corridors and exits discharges to accommodate the additional square footage and subsequent building population increase while not significantly modifying or hindering egress from the existing north side of the building. The design team will treat the existing Straz Center as a separate and unique structure and provide the proper fire separation between the north and south portions of the building. All three floors of the Straz will receive a new sprinkler system.

The new construction will be Type 2B in accordance with the International Building Code and the fire resistance ratings as outlined in Appendix "A" section 3. Incidental use areas will be separated by construction in accordance with the area to be protected. A new fire detection system will be tied into the existing Honeywell system.

**SECTION 4
CONDITIONAL USE PERMIT**

Additional Information Required:	Building or Addition Square Footage: <u>105,387</u> gross square feet, addition and renovation Existing Building Size: <u>70,298</u> gross square feet in area of renovation Site Size: <u>1.79</u> Acres Current # of Employees <u>1,205</u> occs. Anticipated # of New Employees <u>2,007</u> occs. Anticipated Value of Improvements <u>\$31,400,000</u>																																						
Submittal Requirements:	<ul style="list-style-type: none"> ➤ Ten (10) full size scaled copies of Specified Plans indicated below drawn at a standard engineering scale ➤ Developer Site Plan/Conditional Use Permit Checklist (Appendix A) 																																						
If Item to be Reviewed by Plan Commission/Common Council must Submit:	<ul style="list-style-type: none"> ➤ One (1) 8 1/2" x 11" reduction <i>or</i> forty (40) 11" x 17" reductions of the Site/Landscape Plan, Floor Plan and Colored Building Elevations (all sides) ➤ Sample Board containing colored samples of all exterior building materials 																																						
Fees:	<table border="1"> <tr> <td>Level 1</td> <td><= 10,000 sq. ft.</td> <td><= 1 acre</td> <td>\$900 = City Plan Dept. <i>or</i> \$1,025 = CPC/CC</td> </tr> <tr> <td>Level 2</td> <td>10,001 - 50,000 sq. ft.</td> <td>1.01 - 10 acres</td> <td>\$1,175 = City Plan Dept. <i>or</i> \$1,300 = CPC/CC</td> </tr> <tr> <td>Level 3</td> <td>50,001 - 100,000 sq. ft.</td> <td>10.01 - 25 acres</td> <td>\$1,600 = City Plan Dept. <i>or</i> \$1,725 = CPC/CC</td> </tr> <tr> <td>Level 4</td> <td>> 100,001 sq. ft.</td> <td>> 25.01 acres</td> <td>\$2,000 = City Plan Dept. <i>or</i> \$2,125 = CPC/CC</td> </tr> </table>	Level 1	<= 10,000 sq. ft.	<= 1 acre	\$900 = City Plan Dept. <i>or</i> \$1,025 = CPC/CC	Level 2	10,001 - 50,000 sq. ft.	1.01 - 10 acres	\$1,175 = City Plan Dept. <i>or</i> \$1,300 = CPC/CC	Level 3	50,001 - 100,000 sq. ft.	10.01 - 25 acres	\$1,600 = City Plan Dept. <i>or</i> \$1,725 = CPC/CC	Level 4	> 100,001 sq. ft.	> 25.01 acres	\$2,000 = City Plan Dept. <i>or</i> \$2,125 = CPC/CC	<table border="1"> <tr> <td>Building or Addition Size</td> <td>Site size</td> <td>Review Fee</td> </tr> <tr> <td><= 10,000 sq. ft.</td> <td><= 1 acre</td> <td>\$900 = City Plan Dept. <i>or</i> \$1,025 = CPC/CC</td> </tr> <tr> <td>10,001 - 50,000 sq. ft.</td> <td>1.01 - 10 acres</td> <td>\$1,175 = City Plan Dept. <i>or</i> \$1,300 = CPC/CC</td> </tr> <tr> <td>50,001 - 100,000 sq. ft.</td> <td>10.01 - 25 acres</td> <td>\$1,600 = City Plan Dept. <i>or</i> \$1,725 = CPC/CC</td> </tr> <tr> <td>> 100,001 sq. ft.</td> <td>> 25.01 acres</td> <td>\$2,000 = City Plan Dept. <i>or</i> \$2,125 = CPC/CC</td> </tr> </table>	Building or Addition Size	Site size	Review Fee	<= 10,000 sq. ft.	<= 1 acre	\$900 = City Plan Dept. <i>or</i> \$1,025 = CPC/CC	10,001 - 50,000 sq. ft.	1.01 - 10 acres	\$1,175 = City Plan Dept. <i>or</i> \$1,300 = CPC/CC	50,001 - 100,000 sq. ft.	10.01 - 25 acres	\$1,600 = City Plan Dept. <i>or</i> \$1,725 = CPC/CC	> 100,001 sq. ft.	> 25.01 acres	\$2,000 = City Plan Dept. <i>or</i> \$2,125 = CPC/CC	<table border="1"> <tr> <td>Review Fee</td> </tr> <tr> <td>\$900 = City Plan Dept. <i>or</i> \$1,025 = CPC/CC</td> </tr> <tr> <td>\$1,175 = City Plan Dept. <i>or</i> \$1,300 = CPC/CC</td> </tr> <tr> <td>\$1,600 = City Plan Dept. <i>or</i> \$1,725 = CPC/CC</td> </tr> <tr> <td>\$2,000 = City Plan Dept. <i>or</i> \$2,125 = CPC/CC</td> </tr> </table>	Review Fee	\$900 = City Plan Dept. <i>or</i> \$1,025 = CPC/CC	\$1,175 = City Plan Dept. <i>or</i> \$1,300 = CPC/CC	\$1,600 = City Plan Dept. <i>or</i> \$1,725 = CPC/CC	\$2,000 = City Plan Dept. <i>or</i> \$2,125 = CPC/CC
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<ul style="list-style-type: none"> ➤ If building size or addition and gross acreage of the site determine two (2) different fees, the greater of the two fees will be assessed. ➤ Application fee entitles applicant to an initial review and one re-submittal. ➤ Re-submittal fee = \$425 per re-submittal after two (2) permitted reviews. ➤ CUP Amendment = 50% of the applicable fee as determined above. 																																							
Appendices to Review:	<ul style="list-style-type: none"> ➤ All 																																						
Approximate Review Time:	<ul style="list-style-type: none"> ➤ 30 days for Staff Review ➤ 45-60 days for City Plan Commission/Common Council Review 																																						
The conditional use permit plans, <i>prepared to a standard engineering scale</i> , shall be submitted with this application & shall include the following information:																																							
Building Plan:	<ul style="list-style-type: none"> ➤ Layout of building(s) including size and layout of rooms ➤ Design and architecture ➤ Plans and details on fire suppression and/or standpipe ➤ Plans and details on fire detection, fire alarm and other safety devices 																																						
Site Plan (based on a plat of survey)	<ul style="list-style-type: none"> ➤ Legal description of property ➤ Location and footprint of building(s) and structure(s) ➤ Locations of existing and proposed streets, drives, alleys, easements, rights-of-way, parking as required, vehicular and pedestrian access points, and sidewalks ➤ Outline of any development stages ➤ Location and details on any required emergency access roads ➤ A calculation of square footage devoted to building, paving and sidewalks, and landscaped/open space 																																						
Drainage Plan	<ul style="list-style-type: none"> ➤ Existing topography, including spot elevations of existing buildings, structures, high points, and wet areas, with any previous flood elevations ➤ Floodplain boundaries, if applicable ➤ Soil characteristics, where applicable ➤ Proposed topography of the site denoting elevations and natural drainage after construction and any proposed stormwater retention areas 																																						



Carthage College
STRAZ CENTER
Modernization + Expansion

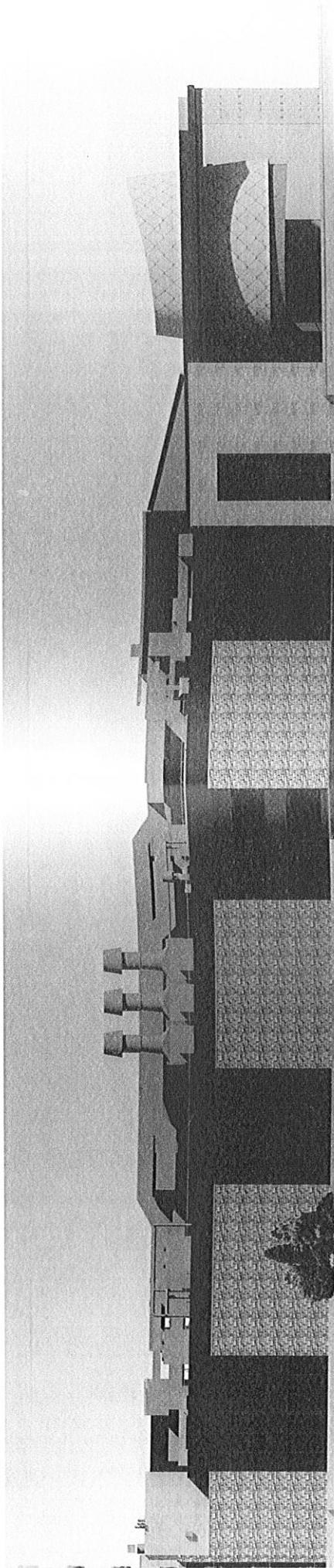




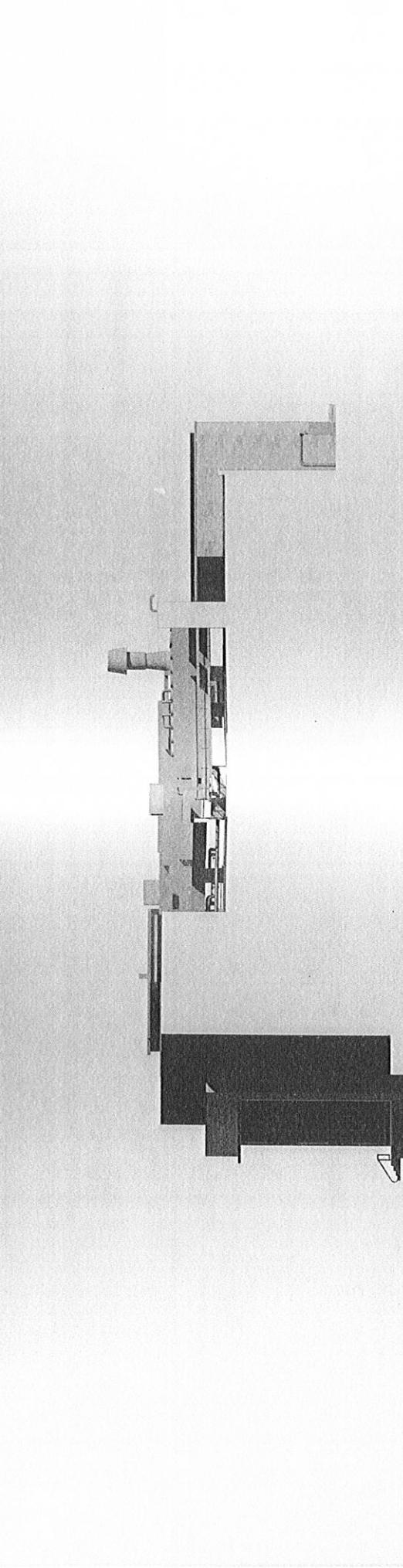
LAKE MICHIGAN PERSPECTIVE

Carthage College
STRAZ CENTER
Modernization + Expansion

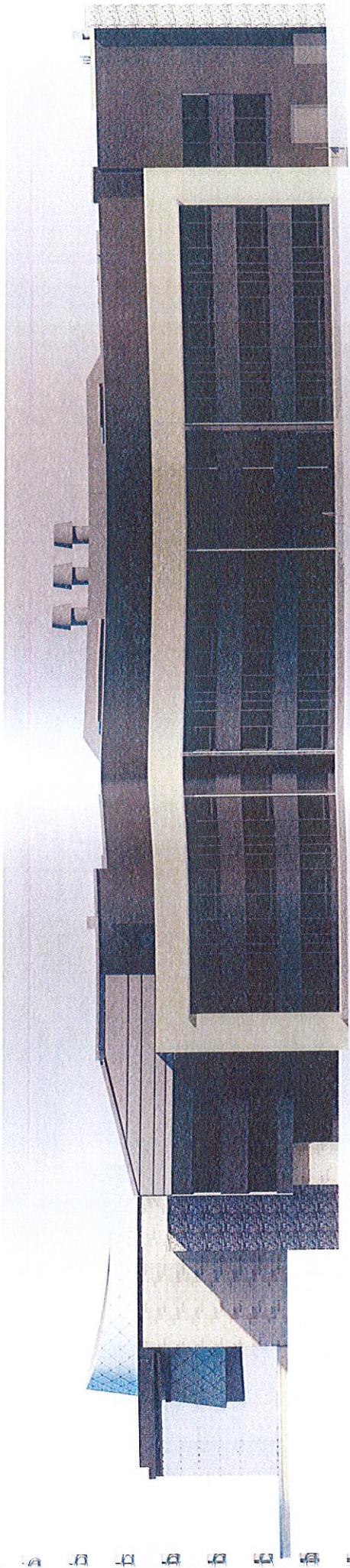




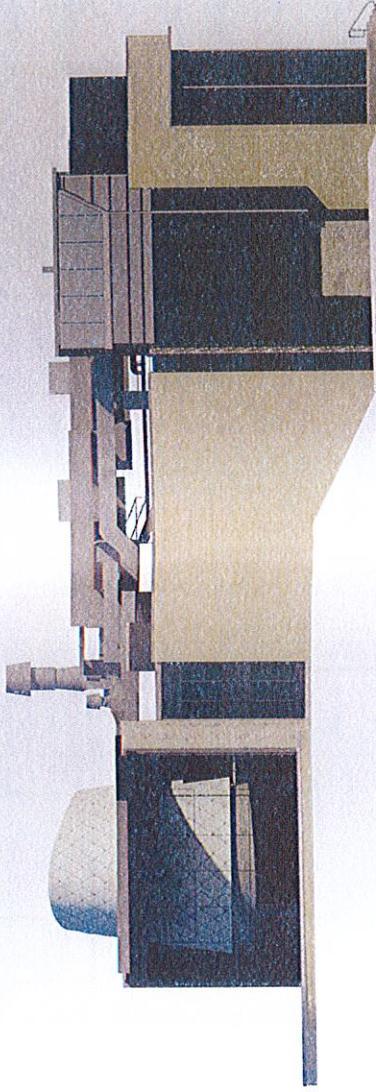
WEST (CAMPUS DRIVE) ELEVATION



NORTH ELEVATION



EAST (LAKE MICHIGAN) ELEVATION



SOUTH ELEVATION

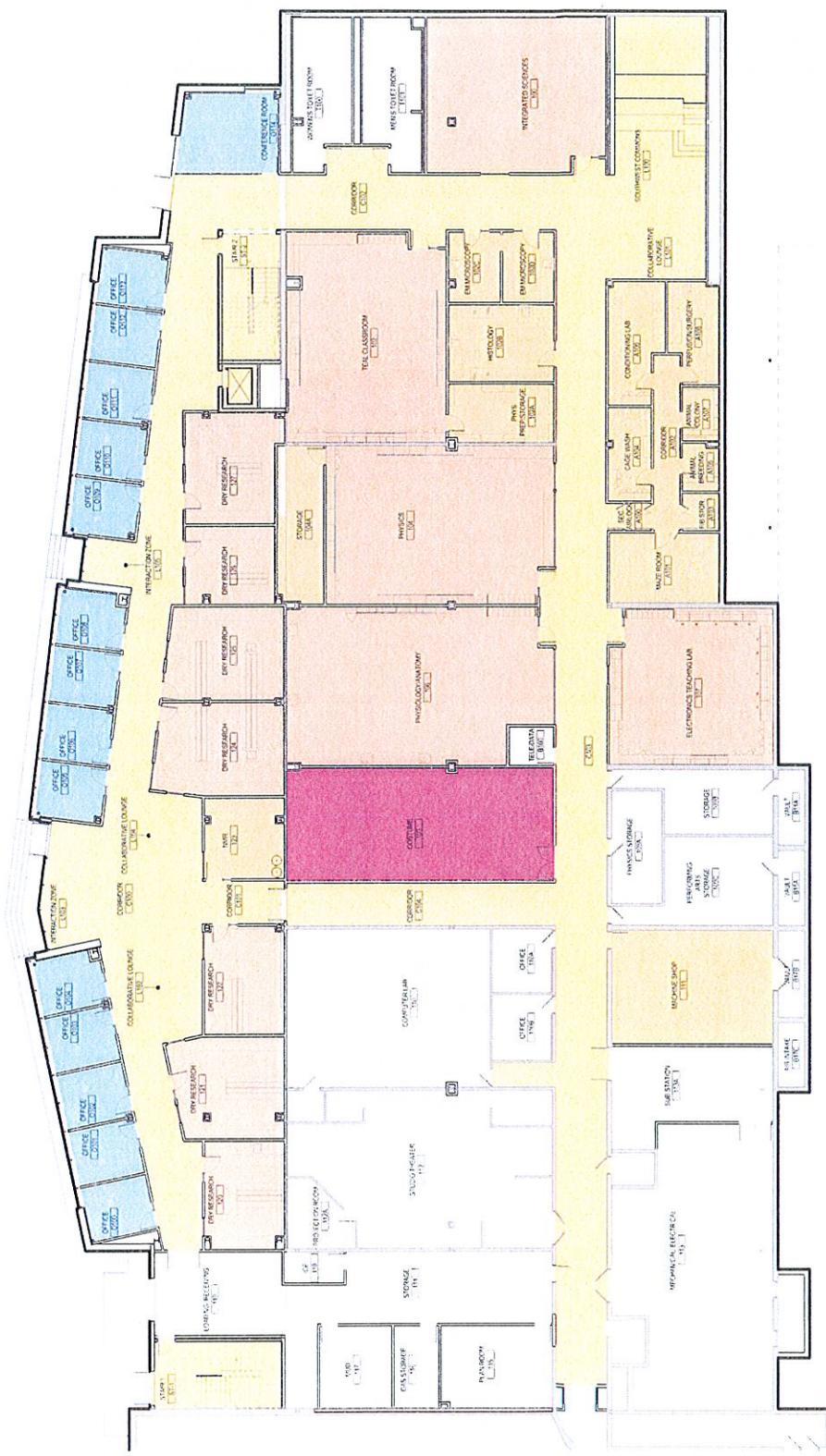


Carthage College
STRAZ CENTER
 Modernization + Expansion

FIRST FLOOR PLAN



- CALCULATION
- LAB
- LAB STORAGE - INSTRUMENTS PAPER
- LOBBY
- METEOROLOGICAL MOUNTS BUILDING SUPPORT
- OFFICE
- RESEARCH
- THEATER



LOWER LEVEL PLAN

Planning & Zoning Division 625 52nd Street - Room 308 Kenosha, WI 53140 262.653.4030	Kenosha City Plan Commission FACT SHEET	August 8, 2013	Item 3
Conditional Use Permit for a 1,949 s.f. restaurant with a drive-thru to be located at 8040 Sheridan Road. (Taco Bell) (District #12) PUBLIC HEARING			

LOCATION/SURROUNDINGS:

Site: 8040 Sheridan Road
 Zoned: B-2 Community Business District

NOTIFICATIONS/PROCEDURES:

The alderman of the district, Alderman Bostrom, has been notified. The Common Council is the final review authority.

ANALYSIS:

- The applicant is proposing to purchase the property at 8040 Sheridan Road, raze the existing structure, the former Eddie's Steakhouse, and construct a new Taco Bell restaurant with a drive-thru. The site would include driveway access off both Sheridan Road and 14th Avenue.
- The proposed exterior materials are stone, brick, EIFS and glass. The front of the building will have aluminum slats mounted over the windows as a decorative feature.
- The drive-thru meets all the required dimensional criteria.
- The applicant will need a Special Exception to the minimum number of off-street parking spaces required. Thirty-eight (38) spaces are required, but only thirty (30) spaces are provided. Two (2) parking spaces may potentially be lost with the requirement of cross access to the north. The Common Council is the review authority for exceptions to off-street parking requirements.
- Staff recommends approval of the exception since this business is primarily drive-thru traffic.
- The plans were sent to City Departments for their review. Their comments are included in the Attached Conditions of Approval.
- The plans generally comply with Sections 4 and 14 of the Zoning Ordinance.

RECOMMENDATION:

A recommendation is made to approve the Conditional Use Permit, subject to the attached Conditions.


 Brian R. Wilke, Development Coordinator


 Jeffrey B. Labahn, Director

Planning & Zoning Division 625 52nd Street Kenosha, WI 53140 262.653.4030	<i>Kenosha City Plan Commission Conditions of Approval</i>	Taco Bell 8040 Sheridan Road	August 8, 2013
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1. The following Conditions of Approval will run with the land and shall be included in a document recorded with the Kenosha County Register of Deeds:
 - a. The applicant shall obtain all required construction permits from the Department of Community Development & Inspections. This includes, but is not limited to Raze, Erosion Control, Building, Plumbing, Electrical and Occupancy permits.
 - b. The applicant shall obtain Driveway, Sidewalk, Street Opening and Parking Lot permits from the Department of Public Works.
 - c. All signs shall comply with Chapter 15 of the Code of General Ordinances and the applicant shall obtain sign permits for all new signs. A separate submission and permits are required for all signs. Sign information provided under the Conditional Use Permit is for reference only and does not constitute sign permit approval. This site shall only be permitted a monument sign.
 - d. The development shall be constructed per the approved plans on file with the Department of Community Development & Inspections, Room 308, 625 52nd Street, Kenosha, WI 53140. Any changes to the approved plans shall require an amendment to the Conditional Use Permit. All changes shall be submitted to the Department of Community Development & Inspections for review and approval.
 - e. Prior to the issuance of any Occupancy permits, all parking areas, drives and designated paved areas shall have the initial lift of asphalt installed. The building exterior shall be completed per the approved plans, the exterior lighting shall be installed and the Conditional Use Permit shall be recorded with the Kenosha County Register of Deeds. All improvements indicated on the plans, including landscaping, shall be installed prior to the issuance of a final Occupancy permit. The recording fees for the Conditional Use Permit shall be submitted by the applicant.
 - f. Compliance with City and State and/or Federal Codes and Ordinances. The buildings shall comply with the current Code standards in effect upon application for a building permit.
 - g. All roof top mechanicals shall be properly screened per Section 14.07 B.10 of the Zoning Ordinance.
 - h. All trash containers shall be stored within the enclosure or building. The applicant/owner shall be responsible for waste collection and removal for the development. The City of Kenosha shall not provide waste collection or removal services or incur any cost in this regard.
 - i. The applicant shall meet all applicable Conditions of Approval and obtain a building permit within six (6) months of Common Council approval of the Conditional Use Permit or the Conditional Use Permit shall be null and void.

Planning & Zoning Division 625 52nd Street Kenosha, WI 53140 262.653.4030	<i>Kenosha City Plan Commission Conditions of Approval</i>	Taco Bell 8040 Sheridan Road	August 8, 2013
--	---	--	----------------

- j. Outdoor display of products is prohibited.
 - k. All vehicles shall be parked within the designated paved areas.
 - l. All improvements, including landscaping, shall be maintained per the approved plans. Any damaged fencing, landscaping or building shall be replaced or reconstructed per the approved plans.
 - m. Cross access shall be provided to adjacent parcels if required at a future date by the City.
 - n. Compliance with the Operational Plan.
 - o. The site is limited to one monument sign.
2. The following conditions of approval shall be satisfied with City Staff prior to the issuance of any construction permits.
- a. The Drainage Plan shall be revised and resubmitted for review and approval addressing comments listed in the Public Works memo dated July 23, 2013.
 - b. The Utility Plan shall be revised and resubmitted for review and approval addressing the comments listed in the Kenosha Water Utility Memo dated July 22, 2013.
 - c. An Exception to the minimum number of off-street parking spaces shall be approved by the Common Council.
 - d. The rear (west) elevation shall be revised to provide the minimum sixty percent (60%) articulation requirement for elevations that abut a public street. Staff suggests the installation of an awning similar to the south elevation.
 - e. Several of the Site Plans and Building Elevation Plans do not match the listed scale on the plan page. The pages may have been reduced during reproduction. Please resubmit all pages at the correct scale.
 - f. A cross access drive shall be provided to the property to the north. The Site Plan shall show a minimum twenty-four (24') foot wide paved driveway installed to the north lot line. The location can be determined by the applicant with approval by the City.
 - g. An Operational Plan shall be provided which indicates hours of operation, number of employees, and anticipated method of trash disposal/removal.



ENGINEERING DIVISION
SHELLY BILLINGSLEY, P.E.
CITY ENGINEER

PARK DIVISION
JEFF WARNOCK
SUPERINTENDENT

FLEET MAINTENANCE
MAURO LENCI
SUPERINTENDENT

STREET DIVISION
JOHN H. PRIJIC
SUPERINTENDENT

WASTE DIVISION
ROCKY BEDNAR
SUPERINTENDENT

DEPARTMENT OF PUBLIC WORKS

MICHAEL M. LEMENS, P.E., DIRECTOR
SHELLY BILLINGSLEY, P.E., DEPUTY DIRECTOR

MUNICIPAL BUILDING · 625 - 52ND ST · RM 305 · KENOSHA, WI 53140
TELEPHONE (262) 653-4050 · FAX (262) 653-4056
EMAIL PUBLICWORKS@KENOSHA.ORG

TO: Brian Wilke, Development Coordinator

FROM: Michael M. Lemens, P.E.
Director of Public Works

Shelly Billingsley, P.E.
City Engineer

[Handwritten signature of Michael M. Lemens]
7-26-13
[Handwritten signature of Shelly Billingsley]

DATE: July 23, 2013
SUBJECT: PLAN REVIEW COMMENTS
Project Description: Taco Bell
Location: 8040 Sheridan Road

Our staff has reviewed the plans for this project. The following comments are provided:

Parking Lot Ordinance Compliance	Sufficient	Deficient	Not Applicable
Parking Lot Paved	X		
Standard Stall Width	X		
Parking Lot Layout	X		
Parking Lot Lighting Shown	X		
Parking Lot Lighting Adequate	X		
Handicapped Parking	X		
Driveway Locations	X		
Driveway Width	X		
Passing Blister or Accel/Decel Lanes			X
Sidewalks Adequate	X		
Drive Thru Lane Design	X		

Public Streets	Sufficient	Deficient	Not Applicable
Geometric Design			X
Pavement Width			X
Pavement Thickness Design			X
Established Grades			X
Plan Details			X
Sidewalks			X
Street Lights			X

Site Grading/Drainage	Sufficient	Deficient	Not Applicable
Drainage Plan		X	
Storm Sewer	X		
Storm Water Detention			X
Drainage Calculations	X		

Project Approval/Permits Needed	Yes	No	Not Applicable
Project Approved for Permitting		X	
Withhold Permits: See Comments	X		
Approve Footing/ Foundation Only (per condition)			X
Parking Lot Permit Required	X		
Driveway Permits Required	X		
Sidewalk Permit Required	X		
Street Opening Permit Required	X		
Stormwater Permit Required			
Erosion Control Required	X		
State Permit Required	X		

Grading & Drainage Comments:

1. Note that the erosion control review is separate from the CUP review and the erosion control plan will be reviewed when the erosion control permit is applied for.
2. Plans must be stamped by a professional engineer registered in Wisconsin.
3. Put the datum of all benchmarks on the plans.
4. Show additional spot elevations at the curb so that an elevation is shown a minimum of every 50' along curb lines and include grades around the bump out on the north side of the parking lot.
5. Show additional spot grades and arrows to show how the grassy area on the west side of the site drains.

6. From inlet #12 the asphalt pavement slopes up at 0.72% to the high point and then down at 0.56% to the low point at inlet #11. The City recommends a minimum of 1% slope on asphalt pavement. It is suggested that inlet #11 be moved west and inlet #12 be moved to the east to achieve a 1% slope on the asphalt pavement.
7. The City suggests allowing no more than 6 inches of ponding before water will overtop a high point in the pavement to prevent damage to vehicles. The rim elevation of inlet #11 is currently 0.84' below the back of sidewalk along Sheridan Road.
8. Specify the proposed diameter of manhole #10 (detail sheet says "see plans" but plans do not specify)

Traffic Comments:

9. Install STOP signs at all exit driveways.
10. Repair all damaged sidewalk.
11. All sidewalks abutting the driveway shall be 8-inch in depth.
12. Driveway approach shall be 8-inch in depth.
13. All driveways shall be standard approach with flares.
14. Remove existing southerly driveway approach on 14th Avenue.
15. Remove asphaltic pavement in the parkway abutting 14th Avenue and replace with sod.

cc: Jeff Hansen
Clement Abongwa
Kile Kuhlmeier
Gerard Koehler

Engineering Services

4401 Green Bay Road
Kenosha WI 53144

Phone (262) 653-4315
Fax (262) 653-4303



"Providing and Protecting Kenosha's Greatest Natural Resource"

MEMO

To: Brian Wilke, Development Coordinator

From: Ian C. Bagley, P.E., Water Engineer

Date: July 22, 2013

Subject: Taco Bell

Location: 8040 Sheridan Road

The Kenosha Water Utility (KWU) has reviewed the submittal for the above referenced project. Further information from the Developer is required before the Utility can complete this review. Please withhold all permits until the following information is submitted.

1. The Demolition Plan (Sheet C-1.0) shall include a note to cut and cap the existing sanitary sewer lateral along the west property line. The plan shall also include a note to cut or peen off the water service being abandoned along the east property line.
2. Show the water meter size and location, including a detail or diagram. The water meter shall have unobstructed access, twelve inches (12") from the inside wall, twelve to twenty-four inches (12"-24") above the floor. The meter shall have a gate valve on the inlet and outlet pipe.
3. All water meters one and one half inches (1-1/2") or greater shall have a bypass with a two way ball valve with locking handle as manufactured by RuB, or approved equal.
4. I spoke with Rich Burke from L+A Architects on July 15, 2013 and he indicated that the restaurant will require a one and one half inch (1-1/2") service. It is the recommendation of KWU that this service be tapped off of the main in 14th Avenue instead of Sheridan Road. The fee for this tap will be \$550 and a sewer connection fee of \$13,126 will also be required.
5. Water service material (main to curb stop) shall be type K copper for all services two inches (2") and smaller.
6. Notes 3 and 5 on the Utility Plan (Sheet C-3.0) make reference to the Village of Pewaukee. These references shall be replaced with "Kenosha Water Utility".
7. The first part of Note 9 on the Utility Plan makes reference to a 4" polyvinyl chloride water service but the plans show a 3/4" service. As mentioned earlier, the architect noted that a one and one half inch (1-1/2") service will be required and the plan shall

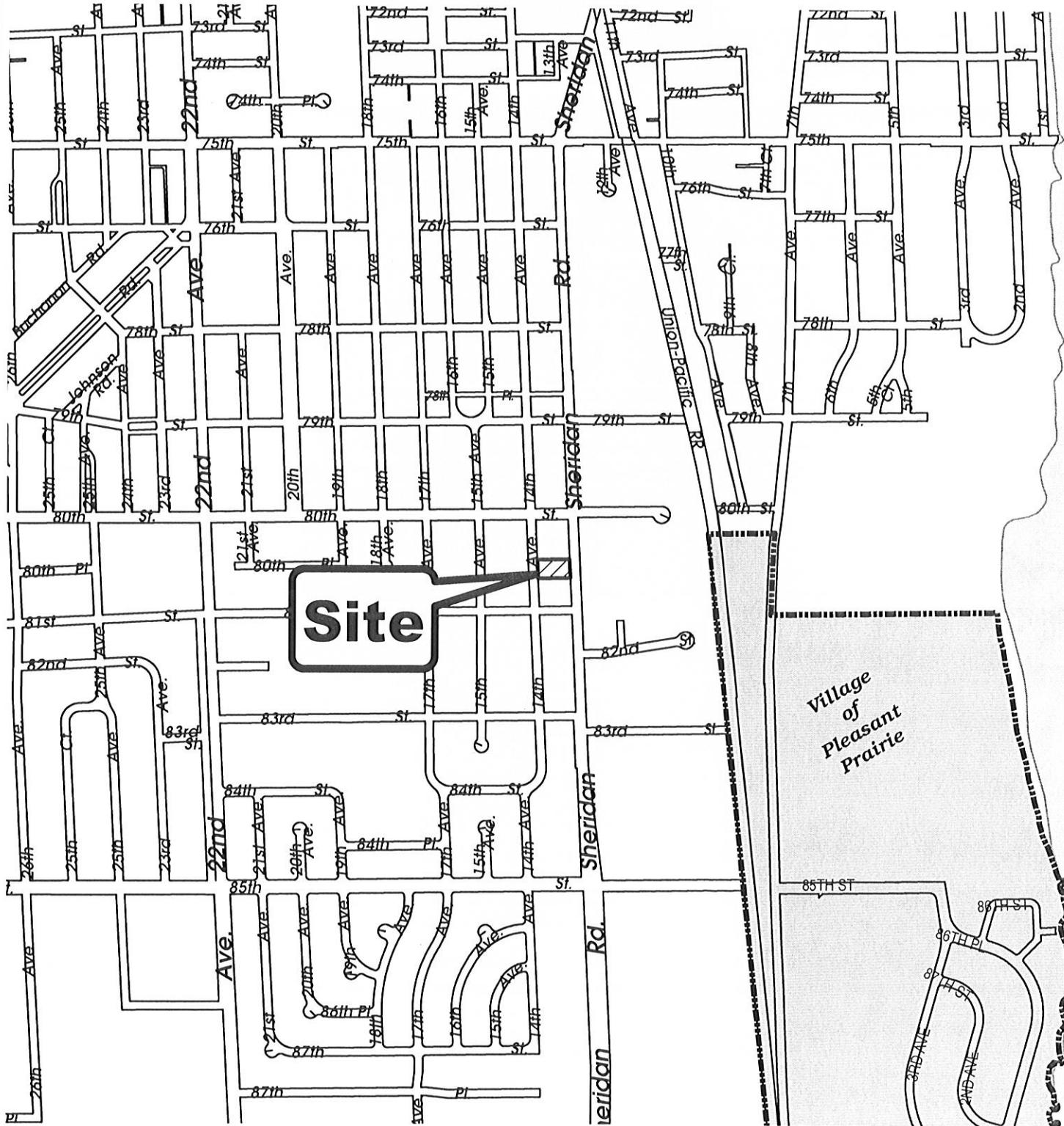
be updated to reflect this change. Also, the reference to the Village of Pewaukee in this note shall be replaced with "Kenosha Water Utility".

8. The third part of Note 9 on the Utility Plan shall be changed to read as follows:
Water services shall have a blue 12 gauge locator wire installed along the entire length. Locator wire shall be brought to the surface in the curb box and at the edge of the building and enclosed in a curb box with "water" on the cover.
9. Add the following note to the Utility Plan:
All water connections to existing water mains shall be completed by KWU, with excavation and backfill by the developer. Developer shall provide 72 hours notice to KWU when connection is to be made.
10. Label invert elevations on all water service stubs and fittings and provide pipe slopes for water service lines.
11. Label invert elevations on all sanitary sewer service stubs and fittings and provide pipe slopes for sanitary sewer service lines.
12. Install a sanitary sewer manhole along the new sanitary sewer service within five feet (5') of the property line. This manhole shall serve as the sampling manhole for the development and the developer shall provide KWU with written authorization to access the structure
13. "Internal / External" manhole seals as manufactured by Adaptor, Inc. shall be provided for each private sanitary sewer manhole installed. A detail for the seal can be found at <http://www.adaptorinc.com/pdf/inexspec.pdf>.
14. The KWU water main and sanitary sewer detail sheets shall be included on a separate sheet and shall not be placed within the designer's title block or include utility plan notes specific to this project. These detail sheets can be found on the KWU website (<http://www.kenoshawater.org/engineering.html>).

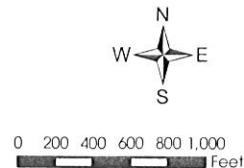
CC: Mr. Robert Carlson, P.E., Director of Engineering Services

City of Kenosha

Vicinity Map Taco Bell CUP



----- Municipal Boundary



**Development Review Application
City of Kenosha, Wisconsin**

MAILING INFORMATION

NAME OF PROJECT: TACO BELL

Check one (1) of the following boxes to indicate the recipient of all correspondence:

<input checked="" type="radio"/>	Name and Address of Applicant (Please print): <u>GREG LAUTZENHEISER</u> <u>L+A ARCHITECTS</u> <u>2430 ROCHESTER COURT</u> <u>TROY, MI. 48083</u>	Phone: <u>(248) 524.4700</u> Fax: <u>(248) 524.9746</u> E-Mail: <u>GLAUTZENHEISER@</u> <u>LAARCHITECTSINC.COM</u>
<input type="radio"/>	Name and Address of Architect/Engineer (Please print): _____ _____ _____	Phone: _____ Fax: _____ E-Mail: _____
<input type="radio"/>	Name and Address of Property Owner (if other than applicant)(Please print): _____ _____ _____	Phone: _____ Fax: _____ E-Mail: _____

PROJECT LOCATION

Location of Development (street address and / or parcel number): 8040 SHERIPAN ROAD

TYPE OF LAND DEVELOPMENT

Check all that apply. Note: Additional information may be required within individual Sections.

<input type="checkbox"/>	Certified Survey Map	Section 1	Page 3
<input type="checkbox"/>	Concept Review (<i>Land Division</i>)	Section 2	Page 4
<input type="checkbox"/>	Concept Review (Multi-Family Residential or Non-Residential)	Section 3	Page 5
<input checked="" type="checkbox"/>	Conditional Use Permit	Section 4	Pages 6 & 7
<input type="checkbox"/>	Developer's Agreement	Section 5	Page 8
<input type="checkbox"/>	Final Plat	Section 6	Pages 9 & 10
<input type="checkbox"/>	Lot Line Adjustment Survey	Section 7	Page 11
<input type="checkbox"/>	Preliminary Plat	Section 8	Pages 12 & 13
<input type="checkbox"/>	Rezoning	Section 9	Pages 14 & 15
<input type="checkbox"/>	Site Plan Review	Section 10	Pages 16 & 17

**PRIOR TO SUBMITTING THIS APPLICATION TO THE DEPARTMENT OF CITY DEVELOPMENT,
PLEASE REVIEW THE APPROPRIATE SECTION(S) FOR FEES, REQUIREMENTS AND APPROPRIATE APPENDICES.**

*Submit this cover page, completed application, applicable section(s) and appendices
along with ALL required plans, information and fees to:*

Department of Community Development & Inspections
Planning Division
625 52nd Street, Room 308
Kenosha, WI 53140

Phone: 262.653.4030
Fax: 262.653.4045

Office Hours:
M - F 8:00 am - 4:30 pm

**SECTION 4
CONDITIONAL USE PERMIT**

Additional Information Required:	Building or Addition Square Footage: <u>1949 SQ. FT.</u> Existing Building Size: <u>N/A</u> Site Size: <u>31,171 SQ. FT.</u> Current # of Employees <u>N/A</u> Anticipated # of New Employees <u>4 FULL TIME</u> + Anticipated Value of Improvements <u>APPROX. \$625,000</u> <u>30 → 40 PART TIME</u>																										
Submittal Requirements:	<ul style="list-style-type: none"> ➤ Ten (10) full size scaled copies of Specified Plans indicated below drawn at a standard engineering scale ➤ Developer Site Plan/Conditional Use Permit Checklist (Appendix A) 																										
If Item to be Reviewed by Plan Commission/Common Council must Submit:	<ul style="list-style-type: none"> ➤ One (1) 8 1/2" x 11" reduction <i>or</i> forty (40) 11" x 17" reductions of the Site/Landscape Plan, Floor Plan and Colored Building Elevations (all sides) ➤ Sample Board containing colored samples of all exterior building materials 																										
Fees:	<table border="1"> <tr><td>Level 1</td></tr> <tr><td>Level 2</td></tr> <tr><td>Level 3</td></tr> <tr><td>Level 4</td></tr> </table>	Level 1	Level 2	Level 3	Level 4	<table border="1"> <tr><th><u>Building or Addition Size</u></th></tr> <tr><td><= 10,000 sq. ft.</td></tr> <tr><td>10,001 - 50,000 sq. ft.</td></tr> <tr><td>50,001 - 100,000 sq. ft.</td></tr> <tr><td>> 100,001 sq. ft.</td></tr> </table>	<u>Building or Addition Size</u>	<= 10,000 sq. ft.	10,001 - 50,000 sq. ft.	50,001 - 100,000 sq. ft.	> 100,001 sq. ft.	<table border="1"> <tr><th><u>Site size</u></th></tr> <tr><td><= 1 acre</td></tr> <tr><td>1.01 - 10 acres</td></tr> <tr><td>10.01 - 25 acres</td></tr> <tr><td>> 25.01 acres</td></tr> </table>	<u>Site size</u>	<= 1 acre	1.01 - 10 acres	10.01 - 25 acres	> 25.01 acres	<table border="1"> <tr><th><u>Review Fee</u></th></tr> <tr><td>\$900 = City Plan Dept. <i>or</i></td></tr> <tr><td>\$1,025 = CPC/CC</td></tr> <tr><td>\$1,175 = City Plan Dept. <i>or</i></td></tr> <tr><td>\$1,300 = CPC/CC</td></tr> <tr><td>\$1,600 = City Plan Dept. <i>or</i></td></tr> <tr><td>\$1,725 = CPC/CC</td></tr> <tr><td>\$2,000 = City Plan Dept. <i>or</i></td></tr> <tr><td>\$2,125 = CPC/CC</td></tr> </table>	<u>Review Fee</u>	\$900 = City Plan Dept. <i>or</i>	\$1,025 = CPC/CC	\$1,175 = City Plan Dept. <i>or</i>	\$1,300 = CPC/CC	\$1,600 = City Plan Dept. <i>or</i>	\$1,725 = CPC/CC	\$2,000 = City Plan Dept. <i>or</i>	\$2,125 = CPC/CC
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<ul style="list-style-type: none"> ➤ If building size or addition and gross acreage of the site determine two (2) different fees, the greater of the two fees will be assessed. ➤ Application fee entitles applicant to an initial review and one re-submittal. ➤ Re-submittal fee = \$425 per re-submittal after two (2) permitted reviews. ➤ CUP Amendment = 50% of the applicable fee as determined above. 																											
Appendices to Review:	<ul style="list-style-type: none"> ➤ All 																										
Approximate Review Time:	<ul style="list-style-type: none"> ➤ 30 days for Staff Review ➤ 45-60 days for City Plan Commission/Common Council Review 																										
<p>The conditional use permit plans, <i>prepared to a standard engineering scale</i>, shall be submitted with this application & shall include the following information:</p>																											
Building Plan:	<ul style="list-style-type: none"> ➤ Layout of building(s) including size and layout of rooms ➤ Design and architecture ➤ Plans and details on fire suppression and/or standpipe ➤ Plans and details on fire detection, fire alarm and other safety devices 																										
Site Plan (based on a plat of survey)	<ul style="list-style-type: none"> ➤ Legal description of property ➤ Location and footprint of building(s) and structure(s) ➤ Locations of existing and proposed streets, drives, alleys, easements, rights-of-way, parking as required, vehicular and pedestrian access points, and sidewalks ➤ Outline of any development stages ➤ Location and details on any required emergency access roads ➤ A calculation of square footage devoted to building, paving and sidewalks, and landscaped/open space 																										
Drainage Plan	<ul style="list-style-type: none"> ➤ Existing topography, including spot elevations of existing buildings, structures, high points, and wet areas, with any previous flood elevations ➤ Floodplain boundaries, if applicable ➤ Soil characteristics, where applicable ➤ Proposed topography of the site denoting elevations and natural drainage after construction and any proposed stormwater retention areas 																										



L + A
ARCHITECTS
INC. A.I.A.
 SUITE 200
 2438 ROCHESTER COURT
 TROY, MICHIGAN 48063
 (313) 781-1100 FAX (313) 781-1101
 (248) 554-4700 (248) 554-4706

PROJECT NO. T13021
JSD Professional Services, Inc.
 Engineers • Architects • Planners
 10000 Woodward Road
 Suite 1000
 Detroit, Michigan 48202
 (313) 963-1100
 www.jsd.com

JSD PROJECT NO. 13-5710

GENERAL NOTES

1. REFER TO SHEET C-1.0 FOR COMPLETE SITE EROSION CONTROL NOTES.
2. REFER TO ADVANCE LAND TITLE REPORT FOR EXISTING CONDITIONS.
3. CONTRACTOR IS RESPONSIBLE TO VERIFY ALL UTILITIES AND TO VERIFY ALL WORK WITHIN PUBLIC RIGHTS-OF-WAY AND MAINTY FOR ALL APPROPRIATE PERMITS.
4. CONTRACTOR SHALL PROTECT CITY SEWERMAINS AND REPLACE IN-WAYS, IF DAMAGED.
5. THE CONTRACTOR SHALL ASSUME SOLE RESPONSIBILITY FOR THE COMPLETION OF ALL WORK FOR THESE AND ADJACENT.
6. CONTRACTOR SHALL VERIFY ALL UTILITIES, INCLUDING BUT NOT LIMITED TO, ELECTRICAL, TELEPHONE, AND WATER UTILITIES, AND SHALL OBTAIN ALL NECESSARY PERMITS FROM THE APPROPRIATE AGENCIES PRIOR TO THE START OF WORK.

LISTED BENCHMARK ELEVATIONS ON ARCHITECTURAL SHEETS: 100'-0" (LISTED) = 614.95' (ACTUAL)

DATE	ISSUE
7-2-2013	SITE PLAN APPROVAL

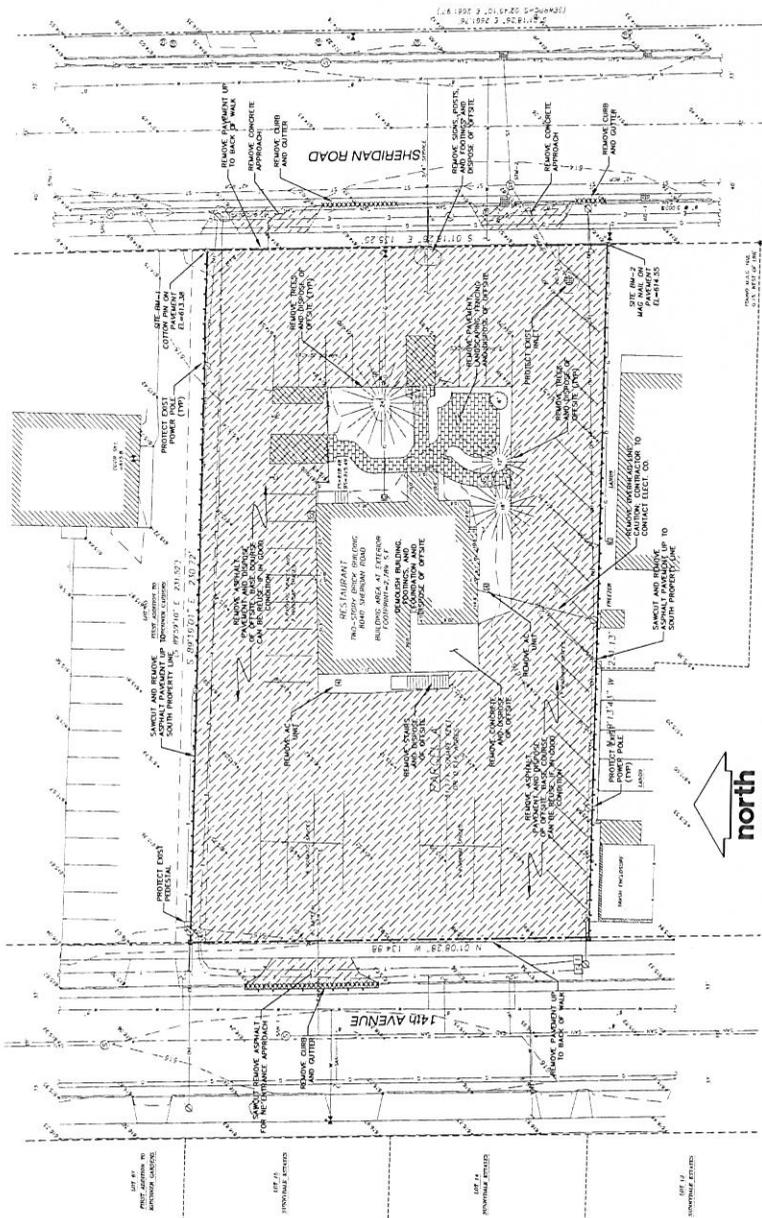
CONTRACT DATE:	MED-N-40
BUILDING TYPE:	DIC 1.0A
PARVISON:	3070D
PROJECT NUMBER:	
STEP NUMBER:	

TACO BELL
 1840 Sheridan Road
 Farmington, MI



MEDIUM40-Ni
SITE DEMOLITION PLAN

C-1.0
 PROJECT DATE: 7-2-13



SHEET INDEX

C-1.0	SITE DEMOLITION PLAN
C-2.0	SITE GRADING AND EROSION CONTROL PLAN
C-3.0	SITE UTILITY PLAN
C-4.0	SITE PAVEMENT PLAN
C-5.0	SITE DETAILS AND NOTES
C-5.1	SITE DETAILS AND NOTES

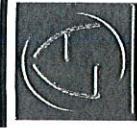
DEVELOPMENTAL LIMITS

PERIODS - 2,020 SF
PERIODS - 25,196 SF (2,790 SF BUILDING)
PERIODS - 12,270 SF
PERIODS - 16,000 SF (2,000 SF BUILDING)



- LEGEND**
- SANITARY MANHOLE
 - CLEAN OUT
 - AIR CONDITION UNIT
 - WHITE OR GAS WALK
 - CURB STOP/SERVICE VALVE
 - STORM MANHOLE
 - SQUARE CASTED INLET
 - CURB INLET
 - GAS REGULATOR METER
 - ELECTRIC MANHOLE
 - AIR CONDITION UNIT
 - WHITE OR GAS WALK
 - CURB STOP/SERVICE VALVE
 - STORM MANHOLE
 - SQUARE CASTED INLET
 - CURB INLET
 - SANITARY SEWER
 - WATER LINE
 - TOWN SEWER
 - NATURAL GAS
 - TELEPHONE
 - UNDERGROUND ELECTRIC
 - UNDERGROUND TELEPHONE

DIGGER'S HOTLINE
 Call 811 or (800) 242-8511
 Michigan Area (248) 632-3700
 www.Diggers hotline.com



L + A
ARCHITECTS
INC. A.I.A.

2430 ROCHSTER COURT
 TROY, MICHIGAN 48063
 PHONE
 (484) 534-1700 (484) 534-1746
 FAX

PROJECT NO. T130021
JSD Professional Services, Inc.
 10000 WOODLAND DRIVE
 ANN ARBOR, MICHIGAN 48106
 PHONE (734) 963-1000
 FAX (734) 963-1001
 JSD PROJECT NO. 130710

CONSTRUCTION SITE EROSION CONTROL

- CONTRACTOR IS RESPONSIBLE TO NOTIFY ENGINEER OF RECORD AND CITY OF ANN ARBOR AND APPROPRIATE CITY OFFICIALS MUST APPROVE ANY CHANGES PRIOR TO EROSION CONTROL MEASURES.
- ALL EROSION CONTROL MEASURES SHALL BE CONSTRUCTED AND MAINTAINED BY THE CONTRACTOR IN ACCORDANCE WITH THE MICHIGAN DEPARTMENT OF HIGHWAY TRANSPORTATION (MDOT) EROSION CONTROL MANUAL, 2008 EDITION, AND THE CITY OF ANN ARBOR EROSION CONTROL MANUAL, WHICH MAY BE NECESSARY TO MEET UNINCORPORATED FIELD CONDITIONS.
- CONTRACTOR SHALL MAINTAIN EROSION CONTROL MEASURES THROUGHOUT THE CONSTRUCTION PERIOD. EROSION CONTROL MEASURES SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD.
- ADDITIONAL EROSION CONTROL MEASURES, AS REQUESTED BY STATE INSPECTOR, SHALL BE INSTALLED WITHIN 24 HOURS OF RECORD.
- INSTALL PERIMETER EROSION CONTROL MEASURES SUCH AS CONSTRUCTION WHIPSTICKS, SLOTTED CURBS, AND SLOTTED CURBS TO PROTECT ADJACENT PROPERTIES/TERRAIN SYSTEMS FROM EROSION. CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN THESE MEASURES THROUGHOUT THE CONSTRUCTION PERIOD.
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DATE	ISSUE
7-2-2013	SITE PLAN APPROVAL

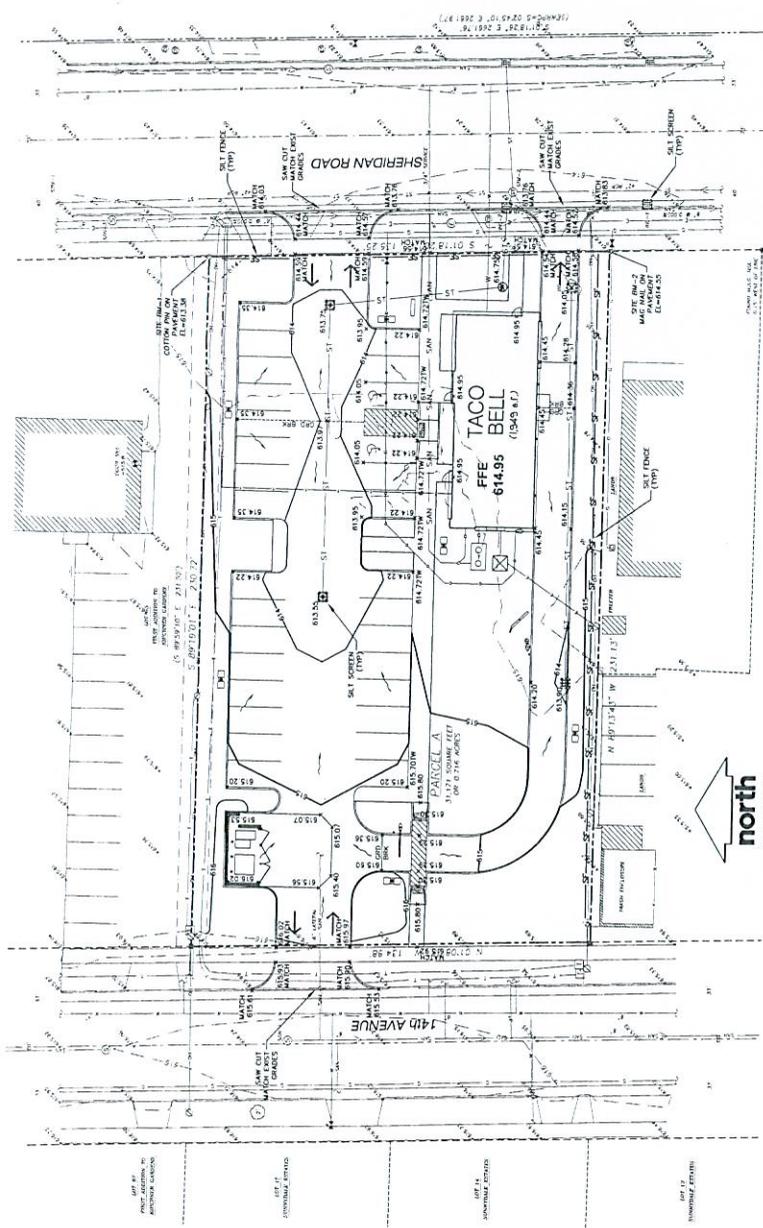
LISTED BENCHMARK ELEVATIONS ON SHEETS 1-1000 (ACTUAL) = 614.95 (ACTUAL)

TACO BELL
 ROAD CORNER WEST



SITE GRADING AND EROSION CONTROL PLAN

C-2.0
 PLOT DATE: 7-2-13



GENERAL GRADING NOTES

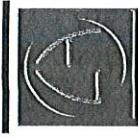
- CONTRACTOR SHALL VERIFY ALL GRADES, MAKE SURE ALL AREAS DRAIN PROPERLY AND STATE OF CONSTRUCTION ACTIVITIES TO THE CHOICES OF RECORD PRIOR TO THE START OF CONSTRUCTION.
- ALL PROPOSED GRADES, SHOWING ARE PROPOSED SURFACE GRADES, FINISHED LOT AND DRIVE GRADES, NOT TOP OF CURB GRADES, UNLESS OTHERWISE SPECIFICALLY NOTED.
- CONTRACTOR IS RESPONSIBLE TO NOTIFY ENGINEER OF RECORD AND CITY OF ANN ARBOR OF ANY CHANGES TO THE EROSION CONTROL AND STORM WATER DRAINAGE SYSTEMS PRIOR TO EROSION FROM THE APPROVED PLAN.
- ALL GRADING OPERATIONS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL BE RESPONSIBLE FOR THE COMPLETION OF THE CONSTRUCTION OF ALL GRADING OPERATIONS INCLUDING SAFETY, ACCESS, AND PROTECTION OF EXISTING UTILITIES AND STRUCTURES. CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN ALL EXISTING UTILITIES AND STRUCTURES THROUGHOUT THE CONSTRUCTION PERIOD.
- CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN ALL EXISTING UTILITIES AND STRUCTURES THROUGHOUT THE CONSTRUCTION PERIOD. CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN THESE UTILITIES AND STRUCTURES THROUGHOUT THE CONSTRUCTION PERIOD.

CONSTRUCTION SITE SEQUENCING

- INSTALL PERIMETER EROSION CONTROL MEASURES THROUGHOUT THE CONSTRUCTION PERIOD.
- CONDUCT GRADING, EXCAVATION, AND OTHER REQS. DETAILED ON SHEET C-1.9.
- INSTALL UTILITY LINES AND PIPING, IMMEDIATELY INSTALL INLET PROTECTION ON OPEN GRADE STRUCTURES.
- INSTALL PERIMETER EROSION CONTROL MEASURES THROUGHOUT THE CONSTRUCTION PERIOD.
- PLACE TOPSOIL, SEED AND MULCH WITH MARY SPECIES 800-30 OR SIMILAR AFTER GRADING IS COMPLETED.
- EROSION CONTROLS WILL NOT BE REMOVED UNTIL VEGETATIVE COVER IS ESTABLISHED. CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN ALL EROSION CONTROL MEASURES THROUGHOUT THE CONSTRUCTION PERIOD.



DIGGERS HOTLINE
 Call 811 (603) 533-8511
 Hours: Monday 8:00 AM - 4:30 PM
 www.diggershotline.com



L+A ARCHITECTS INC. A.I.A.
 2430 ROCHESTER COURT
 SUITE 200
 TROY, MICHIGAN 48063
 PHONE: 481-244-4700 FAX: 481-244-7876
 PROJECT NO. 113021

JSD Professional Services, Inc.
 11000 WOODLAND DRIVE
 WOODLAND, MICHIGAN 48091
 PHONE: 481-244-4700 FAX: 481-244-7876
 JSD PROJECT NO. 13-5710

LISTED BENCHMARK ARCHITECTURAL SHEETS: 100'-0" (LISTED) = 614.95' (ACTUAL)

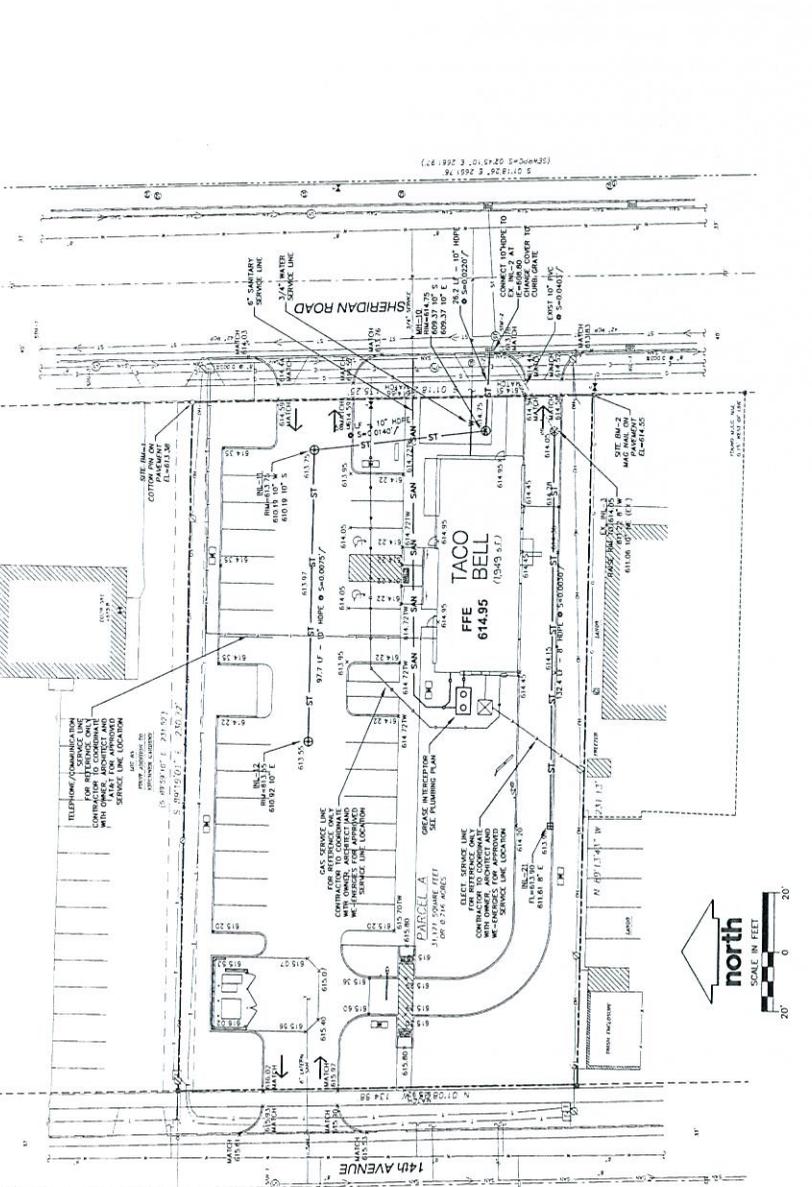
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02-10-04	REVISED
03-10-04	REVISED
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10-10-20	REVISED
11-10-20	REVISED
12-10-20	REVISED

CONTRACT DATE: MED-140
 BUILDING TYPE: DEC 12A
 PLAN/VERSION: 3070D
 SHEET NUMBER: 3070D
 TACO BELL
 1000 Stadium Road
 ANN ARBOR, MI 48106

MEDIUM 40-NI
 SITE UTILITY PLAN

C-3.0
 PROJECT DATE: 12-15-10

- UTILITY NOTES:**
1. ALL UTILITIES ARE SHOWN FOR INFORMATIONAL PURPOSES ONLY AND ARE NOT GUARANTEED. THE CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL CALL "TODD'S INTENT" PRIOR TO ANY CONSTRUCTION.
 2. ALL UTILITY WORK SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND SPECIFICATIONS IS THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN THE LATEST EDITIONS OF ALL APPLICABLE SPECIFICATIONS AND STANDARDS.
 3. ALL UTILITY WORK SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND SPECIFICATIONS IS THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN THE LATEST EDITIONS OF ALL APPLICABLE SPECIFICATIONS AND STANDARDS.
 4. ALL APPLICATIONS AND PERMITS FOR SANITARY SEWER AND WATER MUST BE COMPLETED AND PAID PRIOR TO CONNECTION TO SEWER/WATER SYSTEMS.
 5. ALL SANITARY AND SANITARY CONNECTIONS TO EXISTING LATERALS SHALL BE INSPECTED BY THE CITY OF ANN ARBOR PRIOR TO CONNECTION. ALL CONNECTIONS SHALL BE MADE TO THE EXISTING LATERAL.
 6. LIGHTS, FIRE ALARMS SHALL BE LOCATED ON THE FIELD DURING CONSTRUCTION.
 7. CONNECTIONS SHALL VERIFY ALL ELEVATIONS, LOCATIONS AND SIZES OF SANITARY, WATER AND GAS LATERALS. ALL CONNECTIONS SHALL BE MADE TO THE EXISTING LATERAL.
 8. STORM SEWER SPECIFICATIONS -
 8.1. STORM SEWER PIPE SHALL BE HIGH DENSITY POLYETHYLENE (HDPE) UNLESS NOTED OTHERWISE.
 8.2. ALL SANITARY AND SANITARY CONNECTIONS SHALL BE MADE TO THE EXISTING LATERAL.
 8.3. ALL SANITARY AND SANITARY CONNECTIONS SHALL BE MADE TO THE EXISTING LATERAL.
 8.4. ALL SANITARY AND SANITARY CONNECTIONS SHALL BE MADE TO THE EXISTING LATERAL.
 8.5. ALL SANITARY AND SANITARY CONNECTIONS SHALL BE MADE TO THE EXISTING LATERAL.
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 8.7. ALL SANITARY AND SANITARY CONNECTIONS SHALL BE MADE TO THE EXISTING LATERAL.
 8.8. ALL SANITARY AND SANITARY CONNECTIONS SHALL BE MADE TO THE EXISTING LATERAL.
 8.9. ALL SANITARY AND SANITARY CONNECTIONS SHALL BE MADE TO THE EXISTING LATERAL.
 8.10. ALL SANITARY AND SANITARY CONNECTIONS SHALL BE MADE TO THE EXISTING LATERAL.



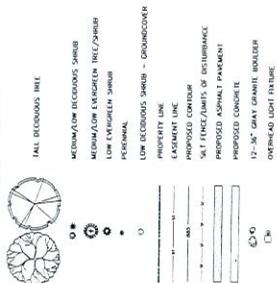
STORM SEWER RATIONAL METHOD

PIPE LOCATION	STRUCTURE CONTRIBUTING AREA		PIPE FLOW		PIPE DATA		PIPE CAPACITY INFORMATION		ELEVATIONS		
	UP-STREAM	DOWN-STREAM	SEWER	RAIN	SEWER	RAIN	ACTUAL FLOW	DESIGN FLOW	MIN (FT)	INVERT (FT)	
UP-STREAM	DOWN-STREAM	SEWER	RAIN	SEWER	RAIN	ACTUAL FLOW	DESIGN FLOW	MIN (FT)	INVERT (FT)	INVERT (FT)	
EX-11	EX-11-1	EX-11-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-12	EX-12-1	EX-12-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-13	EX-13-1	EX-13-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-14	EX-14-1	EX-14-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-15	EX-15-1	EX-15-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-16	EX-16-1	EX-16-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-17	EX-17-1	EX-17-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-18	EX-18-1	EX-18-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-19	EX-19-1	EX-19-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-20	EX-20-1	EX-20-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-21	EX-21-1	EX-21-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-22	EX-22-1	EX-22-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-23	EX-23-1	EX-23-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-24	EX-24-1	EX-24-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-25	EX-25-1	EX-25-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-26	EX-26-1	EX-26-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-27	EX-27-1	EX-27-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-28	EX-28-1	EX-28-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-29	EX-29-1	EX-29-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-30	EX-30-1	EX-30-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-31	EX-31-1	EX-31-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
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EX-35	EX-35-1	EX-35-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-36	EX-36-1	EX-36-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-37	EX-37-1	EX-37-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-38	EX-38-1	EX-38-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-39	EX-39-1	EX-39-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-40	EX-40-1	EX-40-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-41	EX-41-1	EX-41-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-42	EX-42-1	EX-42-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-43	EX-43-1	EX-43-2	0.20	0.40	0.20	0.40	0.20	0.40	613.90	611.22	1.50
EX-44	EX-44-1										



L+A
ARCHITECTS
INC. A.I.A.
 2409 ROCHESTER COURT
 SUITE 200
 TROY, MICHIGAN 48063
 PHONE (481) 541-4700 (481) 541-9746
 FAX (481) 541-4700
 PROJECT NO. 113021

LEGEND (PROPOSED)

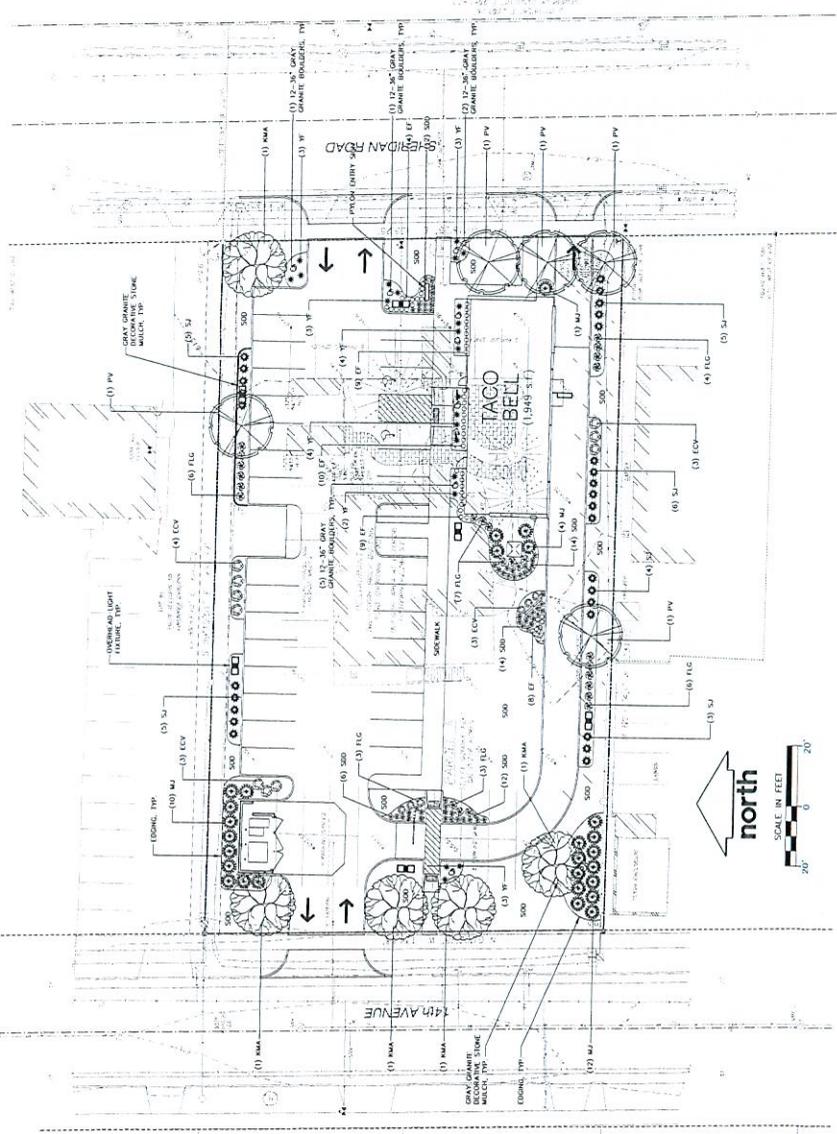


GENERAL NOTES

1. REFER TO THE EXISTING CONDITIONS SURVEY FOR EXISTING CONDITIONS NOTES AND LEGEND.
2. REFER TO CIVIL PLANS FOR PROPOSED EROSION CONTROL, GRADING AND UTILITIES.
3. REFER TO SHEET L2.0 FOR LANDSCAPE DETAILS AND NOTES.
4. IF ANY ERRORS, DISCREPANCIES OR OMISSIONS ARE FOUND, THEY SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER PRIOR TO CONSTRUCTION.
5. CONTRACTOR SHALL OBTAIN NECESSARY PERMITS FOR STREET OPENINGS AND TO WORK WITHIN THE CITY'S LAND IF APPLICABLE.
6. ALL WORK IN THE ROW AND PUBLIC PLACEMENTS SHALL BE IN ACCORDANCE WITH THE CITY OF TROY REQUIREMENTS.
7. NO SITE GRADING OUTSIDE OF PROPOSED LIMITS OF DISTURBANCE.

PARKING LOT SITE INFORMATION BLOCK

SITE ADDRESS: 1800 SHERIDAN ROAD
 NUMBER OF PAVED SPACES: 100
 NUMBER OF UNPAVED SPACES: 100
 TOTAL NUMBER OF SPACES: 200
 TOTAL SQUARE FOOTAGE OF PAVING: 1,949 SQ. FT.
 USE OF PROPERTY: RESTAURANT
 GROSS SQUARE FEET OF RETAIL: N/A
 NUMBER OF PARTIQUES OR PRODUCTION AREA: N/A
 CAPACITY OF RESTAURANT/PRODUCTION AREA: N/A
 SMALL CAR: N/A
 LARGE CAR: N/A
 TOTAL: 200 PARKING SPACES
 ACCESSIBLE: 7 PARKING SPACES
 DETAIL: 10% OF ALL OF ACCESSIBLE PARKING SPACES
 1. TREES, REFERRED TO BY TREE #, PROVIDED.
 2. TREES, REFERRED TO BY TREE #, PROVIDED.
 3. TREES, REFERRED TO BY TREE #, PROVIDED.



LISTED BENCHMARK

CONTRACT DATE: MEDIN 40
 BUILDING TYPE: EEC 12 A
 PLAN REGION: 30763
 SITE NUMBER:
 STORE NUMBER:
 TACO BELL
 1800 Sheridan Road
 Troy, MI 48063

ISSUE

7/2013 SITE PLAN APPROVAL

CONTRACT DATE

7/2013

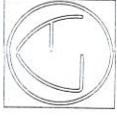


L 1.0
 PROJECT DATE: 7/2/13

LANDSCAPE PLANT SCHEDULE

SYM	QTY	COMMON NAME	BOTANICAL NAME	SIZE	ROOT
MEDM DECIDUOUS TREES	1	Small Crabapple	Malus domestica	2'-0" x 1'-0"	BB
SMALL DECIDUOUS SHRUBS	13	Small Crabapple	Malus domestica	2'-0" x 1'-0"	BB
ECY	13	Small Crabapple	Malus domestica	2'-0" x 1'-0"	BB
PERENNIALS	28	Small Crabapple	Malus domestica	2'-0" x 1'-0"	BB
GROUND COVER	50	Small Crabapple	Malus domestica	2'-0" x 1'-0"	BB
ORNAMENTAL GRASSES	75	Small Crabapple	Malus domestica	2'-0" x 1'-0"	BB





L + A
ARCHITECTS
INC. A.I.A.
 2430 ROCHESTER COURT
 SUITE 300
 TROY, MICHIGAN 48063
 PHONE: (482) 244-4400 FAX: (482) 244-4401
 (482) 244-4100 (482) 244-9100
 PROJECT NO. T13021

LISTED BENCHMARK
 ARCHITECTURE
 ARCHITECTS
 SHEETS: 0'-0" (LISTED)
 = 614.95' (ACTUAL)

DATE: _____
 ISSUE: _____

7/2021 SITE PLAN APPROVAL

CONTRACT DATE	MEDIAN #0
BUILDING TYPE	DEC. 10 A
PLAN REVISION	2020A
DATE	2020A
SHEET NUMBER	

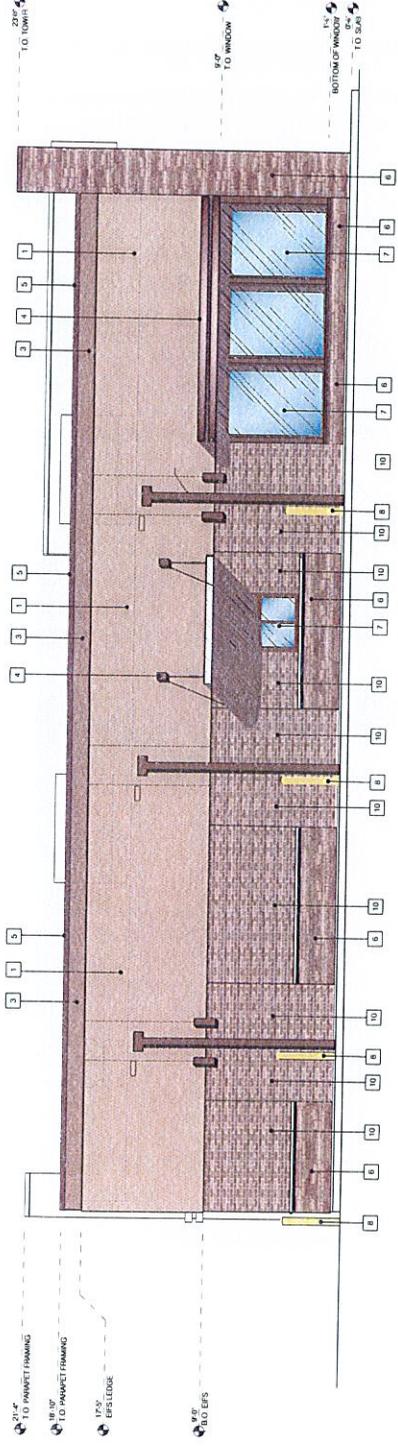
TACO BELL
 1840 University Blvd
 Ann Arbor, MI



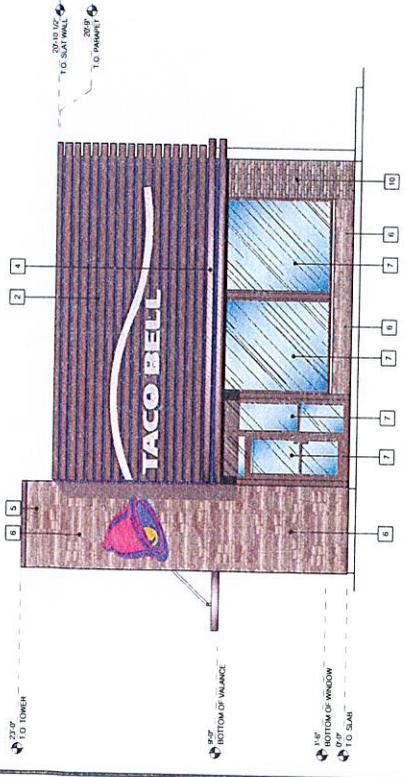
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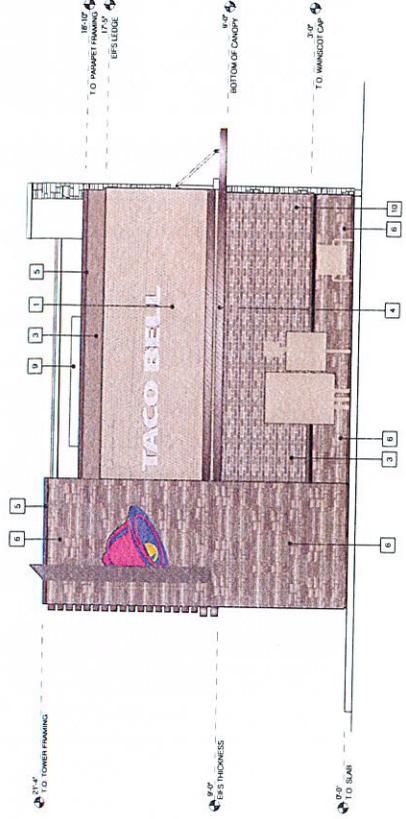
PLotted Date: 7/21/21



RIGHT SIDE ELEVATION 1/4" = 1'-0"



FRONT ELEVATION 1/4" = 1'-0"



REAR ELEVATION 1/4" = 1'-0"

Planning & Zoning Division 625 52nd Street Kenosha, WI 53140 262.653.4030	Kenosha City Plan Commission FACT SHEET	August 8, 2013	Item 4
By the City Plan Commission - To Repeal and Recreate Section 3.31 H. of the Zoning Ordinance for the City of Kenosha Regarding Garage Door Placement. PUBLIC HEARING			

LOCATION/SURROUNDINGS:

Site: City-Wide

NOTIFICATIONS/PROCEDURES:

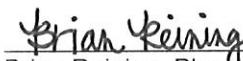
The Common Council is the final review authority.

ANALYSIS:

- The proposed Amendments are intended to clarify the current requirements relating to the placement of garages in the TRD-1 Traditional Single and Two Family Residential District.
- Information regarding the TRD-1 District is found in Section 3.31 H. of the Zoning Ordinance.

RECOMMENDATION:

For Commission review and recommendation.


 Brian Reining, Planner II


 Jeffrey B. Labahn, Director

DRAFT 07/12/13

SPONSOR: CITY PLAN COMMISSION

**TO REPEAL AND RECREATE SECTION 3.31 H. OF
THE ZONING ORDINANCE FOR THE CITY OF KENOSHA
REGARDING GARAGE DOOR PLACEMENT.**

The Common Council of the City of Kenosha, Wisconsin, do ordain as follows:

Section One: Section 3.31 H. of the Zoning Ordinance of the City of Kenosha,

Wisconsin, is hereby repealed and recreated as follows:

~~**H. Garage Door Placement.** The garage wall(s) that includes a garage door opening facing a public or private street, shall not comprise more than fifty (50%) percent of the principal building width, measured along the ground level of the front facade. In addition, one of the following requirements shall be met:~~

~~1. The garage wall(s) that includes a garage door opening(s) facing a public or private street shall not extend closer to the front lot line than the living portion(s) of the principal building, unless the garage door opening(s) is not oriented toward the public or the private street; or,~~

~~2. The garage wall(s) that includes a garage door opening facing a public or private street shall be allowed to protrude beyond the living portions of the principal building up to the end of an unenclosed and covered porch, stoop, patio, portico and pergola, provided that at least fifty (50%) percent of the width of the garage wall(s), measured along the ground level of the front facade, is comprised of an unenclosed and covered porch, stoop, portico, or pergola for the living portions of the principal building.~~

~~3. The garage wall(s) where a garage door opening that faces a public or private street is located closer to the front lot line than the closest living portion of the principal building shall include an unenclosed and covered porch, stoop, portico, patio, or pergola of at least twenty-five (25) square feet in area along the front facade. Such entrance features shall be integrated into the design of the principal building and decorative posts, columns, and piers, including railings, balustrades, brackets, trellises, and other similar elements shall be used so as to de-emphasize any garage doors to the extent possible.~~

~~**H. Front-Facing Garages.** The width of front-facing garages shall not comprise more than fifty (50%) percent of the width of the principal building as measured along the ground level of the front facade containing the primary entrance. In addition, one of the following requirements shall be met:~~

~~1. Front-facing garages shall not extend closer to the front lot line than the liveable space of the principal building; or~~

~~2. Front-facing garages shall not extend closer to the front lot line than an unenclosed and covered porch and/or pergola provided that the width of the unenclosed and covered porch and/or pergola is at~~

least fifty (50%) percent of the width of the front-facing garage wall as measured along the ground level of the front facade containing the primary entrance. The unenclosed and covered porch and/or pergola shall be a minimum of twenty-five (25 sq.) square feet and incorporate the use of decorative posts, columns, piers, railings, balustrades, brackets, trellises, or other similar design elements to minimize the appearance of the front-facing garage to the extent possible.

Section Two: This Ordinance shall become effective upon passage and publication.

ATTEST: _____ City Clerk

APPROVED: _____ Mayor

Passed:

Published:

Drafted By:
JONATHAN A. MULLIGAN
Assistant City Attorney

Planning & Zoning Division 625 52nd Street Kenosha, WI 53140 262.653.4030	Kenosha City Plan Commission FACT SHEET	August 8, 2013	Item 5
By the City Plan Commission - To Repeal and Recreate Section 3.32 H. of the Zoning Ordinance for the City of Kenosha Regarding Garage Door Placement. PUBLIC HEARING			

LOCATION/SURROUNDINGS:

Site: City-Wide

NOTIFICATIONS/PROCEDURES:

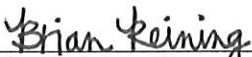
The Common Council is the final review authority.

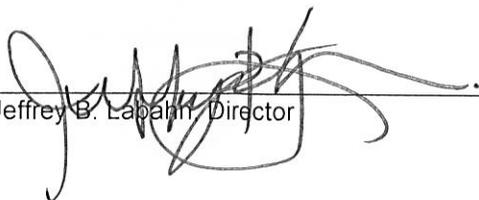
ANALYSIS:

- The proposed Amendments are intended to clarify the current requirements relating to the placement of garages in the TRD-2 Traditional Multiple Family Residential District.
-
- Information regarding the TRD-2 District is found in Section 3.32 H. of the Zoning Ordinance.

RECOMMENDATION:

For Commission review and recommendation.


 Brian Reining, Planner II


 Jeffrey B. Labahn, Director

DRAFT 07/26/13

SPONSOR: CITY PLAN COMMISSION

**TO REPEAL AND RECREATE SECTION 3.32 H. OF
THE ZONING ORDINANCE FOR THE CITY OF KENOSHA
REGARDING GARAGE DOOR PLACEMENT**

The Common Council of the City of Kenosha, Wisconsin, do ordain as follows:

Section One: Section 3.32 H. of the Zoning Ordinance of the City of Kenosha,

Wisconsin, is hereby repealed and recreated as follows:

~~**H. Garage Door Placement.** The garage wall(s) that includes a garage door opening facing a public or private street shall not comprise more than fifty (50%) percent of the principal building width, measured along the ground level of the front facade. In addition, one of the following requirements shall be met:~~

~~1. The garage wall that includes a garage door opening facing a public or private street shall not extend closer to the front lot line than the living portions of the principal building, unless the garage door opening is not oriented toward the public or private street; or,~~

~~2. The garage wall that includes a garage door opening(s) facing a public or private street shall be allowed to protrude beyond the living portions of the principal building up to the end of an unenclosed and covered porch, stoop, patio, portico and pergola, provided that at least fifty (50%) percent of the width of the garage wall(s), measured along the ground level of the front facade is comprised of an unenclosed and covered porch, stoop, portico, or pergola for the living portions of the principal building.~~

~~3. The garage wall where a garage door opening that faces a public or private street is located closer to the front lot line than the closest living portion of the principal building shall include an unenclosed and covered porch, stoop, portico, patio, or pergola of at least twenty-five (25) square feet in area along the front facade. Such entrance features shall be integrated into the design of the principal building and decorative posts, columns, and piers, including railings, balustrades, brackets, trellises, and other similar elements shall be used so as to de-emphasize any garage doors to the extent possible.~~

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1. Front-facing garages shall not extend closer to the front lot line than the liveable space of the principal building; or

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least fifty (50%) percent of the width of the front-facing garage wall as measured along the ground level of the front facade containing the primary entrance. The unenclosed and covered porch and/or pergola shall be a minimum of twenty-five (25 sq.) square feet and incorporate the use of decorative posts, columns, piers, railings, balustrades, brackets, trellises, or other similar design elements to minimize the appearance of the front-facing garage to the extent possible.

Section Two: This Ordinance shall become effective upon passage and publication.

ATTEST: _____ City Clerk

APPROVED: _____ Mayor

Passed:

Published:

Drafted By:
JONATHAN A. MULLIGAN
Assistant City Attorney

<i>Planning & Zoning Division 625 52nd Street Kenosha, WI 53140 262.653.4030</i>	<i>Kenosha City Plan Commission FACT SHEET</i>	<i>August 8, 2013</i>	<i>Item 6</i>
<i>Review of Wisconsin Highway 50 Access Management Vision.</i>			

LOCATION/SURROUNDINGS:

Site: STH 50/75th Street from 43rd Avenue to I-94

NOTIFICATIONS/PROCEDURES:

This is for review and comment on the City Plan Commission level.

ANALYSIS:

- The Plan was prepared by the Wisconsin Department of Transportation (WisDOT) with input from the City of Kenosha, Village of Pleasant Prairie and County of Kenosha. The Plan is intended to be a joint plan to limit access to STH 50.
- The Plan designates where future access points to STH 50 will be located as well as identifying where existing access may be removed or relocated.
- There are locations for future frontage roads. Some of these roads will be constructed as development allows. Others may require Official Map Amendments, which would occur at a later date.

RECOMMENDATION:

For Commission review and comment.



Rich Schroeder, Deputy Director

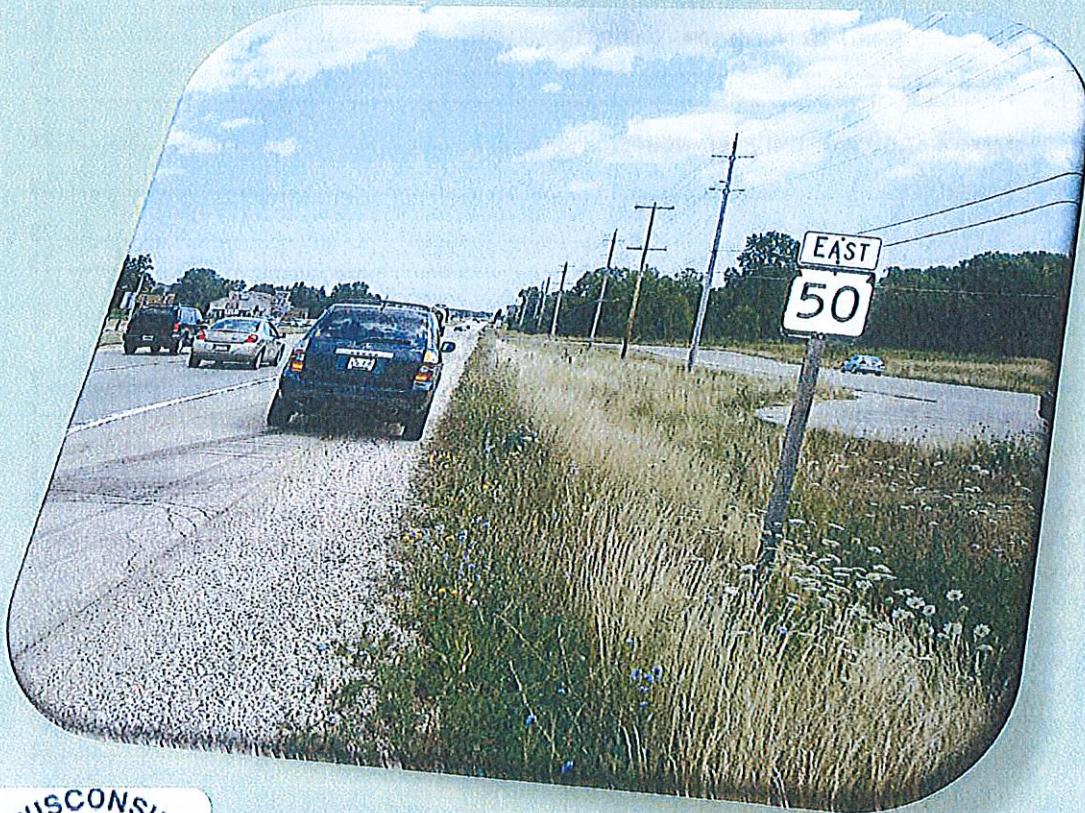
Jeffrey B. Labahn, Director

Wisconsin Highway 50 Access Management Vision

I-94 to 43rd Avenue

Kenosha County

January 2012



VILLAGE OF
PLEASANT PRAIRIE



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WIS 50 Access Management Vision

**I-94 to 43rd Avenue
Kenosha County**

1.0 Introduction

In 1987, officials from the then Town of Pleasant Prairie, the City and County of Kenosha, and the Wisconsin Department of Transportation (WisDOT) jointly developed the “Highway Access and Development Plan” for WIS 50 between I-94 and 60th Avenue.

The purpose of the WIS 50 Access Management Vision is to provide the Wisconsin Department of Transportation (WisDOT) and local units of government a shared, long-range access management vision of the corridor. The Vision will be used as a comprehensive and collaborative tool to evaluate future access requests as development and redevelopment occurs adjacent to WIS 50. The Vision will also provide a guide for local officials to determine the optimum location for developments of differing type, scale and traffic generation.

Transportation systems must balance the needs of preserving mobility and providing access to local land uses. Access management techniques have been used increasingly across the nation and Wisconsin by both state and local units of government in an effort to preserve and prolong safe and efficient traffic operations on the highway system.

WisDOT initiated an update to the 1987 access plan because of the WIS 50 transportation improvement effort and increased interest in changing land use along the corridor. Anticipated pressures for future development and higher projected traffic volumes for WIS 50 drive the need for this project.

The Vision’s development was a result of a collaborative effort, developed with the participation of the local communities and Kenosha County. The overall goal of the Vision is to provide recommendations that will address long-term access and traffic needs along the corridor. It is anticipated that continued, long-term implementation of the Vision would meet the demands of future traffic and sustain an economically attractive area long into the future.

This report serves to update the access plan that was completed in 1987 for this corridor. The update will address changes in the corridor since 1987 relating to

land use, growth trends and future traffic volumes. The main changes in the corridor since the 1987 plan are as follows:

- Higher than anticipated levels of development in the western part of the corridor
- Changes in anticipated land use
- Higher than anticipated traffic volumes
- Update changes to highway since 1987 that includes new and proposed signal locations, median openings and access locations
- Update of access management techniques
- Expanded corridor limits from 60th Street to 43rd Avenue

The update to the access was done concurrently with the WIS 50 corridor expansion study, which resulted in an Environmental Assessment, functional plans and Design Study Report (DSR).

This update has been renamed to be the Wisconsin Highway 50 Access Management Vision. This report and attached maps represent the Vision in its entirety.

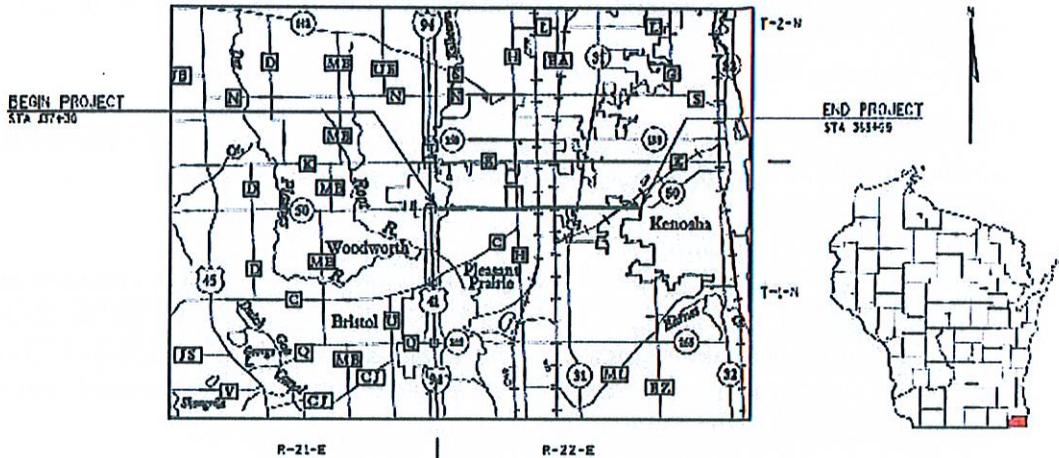
1.1 Project Overview

Project Limits

The vision limits are from I-94 to 43rd Avenue in Kenosha County, a distance of approximately five (5) miles.

The Vision's influence area extends ½ mile north and south of the proposed new alignment of WIS 50 for a total corridor width of approximately one mile. Existing properties, driveways, local roads, known developments and long-range land use plans have been considered in the development of this Vision.

Figure 1 - Vision Location



Current Jurisdiction

WIS 50 is under WisDOT jurisdiction. The project corridor lies within the City of Kenosha and the Village of Pleasant Prairie.

Existing Land Use

Much of the corridor is urbanized. Land use along the corridor consists mostly of residential and business development. A brief summary of land use development since 1987 is provided in the following paragraphs.

Kilbourn Road Ditch to 104th Avenue

On the north side of WIS 50, the Riverwood residential complex, the Aurora Health Care complex and the First Assembly of God Church/school have been constructed.

104th Avenue to 88th Avenue

The Cornerstone Parkway Offices and Condominiums and the Whitecaps subdivision residential development are located on the north side of WIS 50. On the south side of WIS 50, VK Development has constructed Prairie Ridge, a large scale, mixed land use development. The 400-acre site bounded by WIS 50, 88th Avenue, CTH C and 104th Avenue represents the largest single development in the corridor. The site is projected to generate an additional 15,000 vehicles per day on WIS 50. The TIA prepared as part of VK Development's plans recommended installation of a new traffic signal at 94th Avenue which is now installed.

88th Avenue to the railroad overpass (approximately STA 248+00)

This area has developed primarily with commercial uses as was anticipated in the 1987 plan.

Railroad overpass (approximately STA 248+00) to WIS 31

The expansion of commercial development (Southport Plaza) has occurred as planned.

WIS 31 to 60th Avenue

The majority of the land use has been developed as commercial as anticipated. Plans for select new developments have been identified.

60th Avenue to 43rd Avenue

The existing land use in this area is mainly small to mid-size commercial development with two exceptions being a Pick 'n Save grocery store on the north side of WIS 50 and Palmen Motors to the south. Between 45th Avenue and 43rd Avenue, abutting lands are residential with single family homes to the north and a mobile home park to the south.

Access Approval Process

New access or changes in type of access on WIS 50 are considered by WisDOT on a case-by-case basis and follow a permitting process. Under the permitting process, construction of new driveways and/or a change in the status of driveways along WIS 50 requires WisDOT approval. At this time, approvals are made on per request (per parcel) basis.

1.2 Project Stakeholders

The Department of Transportation began the WIS 50 Access Management Vision effort in collaboration with Kenosha County and local municipalities.

Kenosha County

Andy Buehler

Division of Planning Operations Director

Gary Sipsma

Division of Highways Director

City of Kenosha

Rich Schroeder

Assistant City Planner

Michael Lemens

Director/City Engineer

Jeff Labahn

City Development Director

Village of Pleasant Prairie

Jean Werbie-Harris

Community Development Director

Michael Spence

Village Engineer

Michael Pollocoff

Village Administrator

WisDOT

Robert Elkin

Project Supervisor

Vida Shaffer

Project Manager

The vision development process consisted of comprehensive data collection, local official involvement and the application of access management principles. The project stakeholders concluded their work on the Vision with the culmination of this document released **January 2012**.

1.3 Project Input Process

Public involvement has been an important part of the study process. Several meetings with local residents and business owners were scheduled throughout the course of the study to obtain comments and input. The primary comment periods were encouraged at the public informational meetings which were held on the following dates:

PIM #1 – September 23, 1998

PIM #2 – February 25, 1999

PIM #3 – June 30, 2005

PIM #4 – August 29, 2006

2.0 Purpose and Need

The WIS 50 Access Management Vision was undertaken to develop a collaborative long-range vision for the corridor. It provides for a safe and efficient transportation system and balances the increasing access needs with the future mobility of WIS 50. Specifically, the Vision will address the following issues:

- Corridor growth and development pressures
- Increasing traffic, safety, and operational issues
- Improved intergovernmental cooperation
- Common long-range corridor vision

To address the needs, the Access Management Vision's purpose includes strategies and recommendations to:

- Manage existing access.
- Manage new access due to new or redeveloped land use changes.
- Manage traffic safely and efficiently as growth occurs.
- Plan a supportive local road network.
- Improve coordination between WisDOT and local units of government.

As the Southeast region's official metropolitan planning organization, SEWRPC prepares an advisory plan to guide transportation planning decisions. This plan, completed in 2006, and updated in 2010, makes the following recommendation for WIS 50 in Kenosha County:

-
- Widening and/or other improvement to provide significant additional capacity for (or expansion of) WIS 50 from four to six lanes for the entire limits of the corridor.

2.1 Corridor Growth and Development Pressures

Several new developments that occurred since 1987 have been identified in the “Existing Land Use” discussion in Section 1.1 of this report.

Future Land Use (see Appendix 5.1 for future land use maps)

Kilbourn Road Ditch to 104th Avenue

On the south side of WIS 50, the existing residential parcels abutting the highway are anticipated to redevelop into a mixed use development in combination with the currently large, vacant tract of land immediately south of the existing row of residential properties. Conceptual plans have been developed for the area, but no firm development commitments have been identified to date.

104th Avenue to 88th Avenue

Numerous currently vacant lots of the VK Development are expected to develop into commercial uses, including a large tract of land in the southeast quadrant of 94th Avenue and WIS 50.

88th Avenue to the railroad overpass (approximately STA 248+00)

Existing wetlands south of STH 50 limit the potential for expansion in this area. There is limited potential for redevelopment of remaining land which is currently predominantly commercial.

Railroad overpass (approximately STA 248+00) to WIS 31

The existing residential area south of WIS 50 is expected to convert to commercial use. This is consistent with the 1987 plan which called for the land use conversion once Southport Plaza experienced further growth.

WIS 31 to 60th Avenue

Larger land tracts south of 76th Street and east of WIS 31 are expected to accommodate large-scale commercial/industrial development.

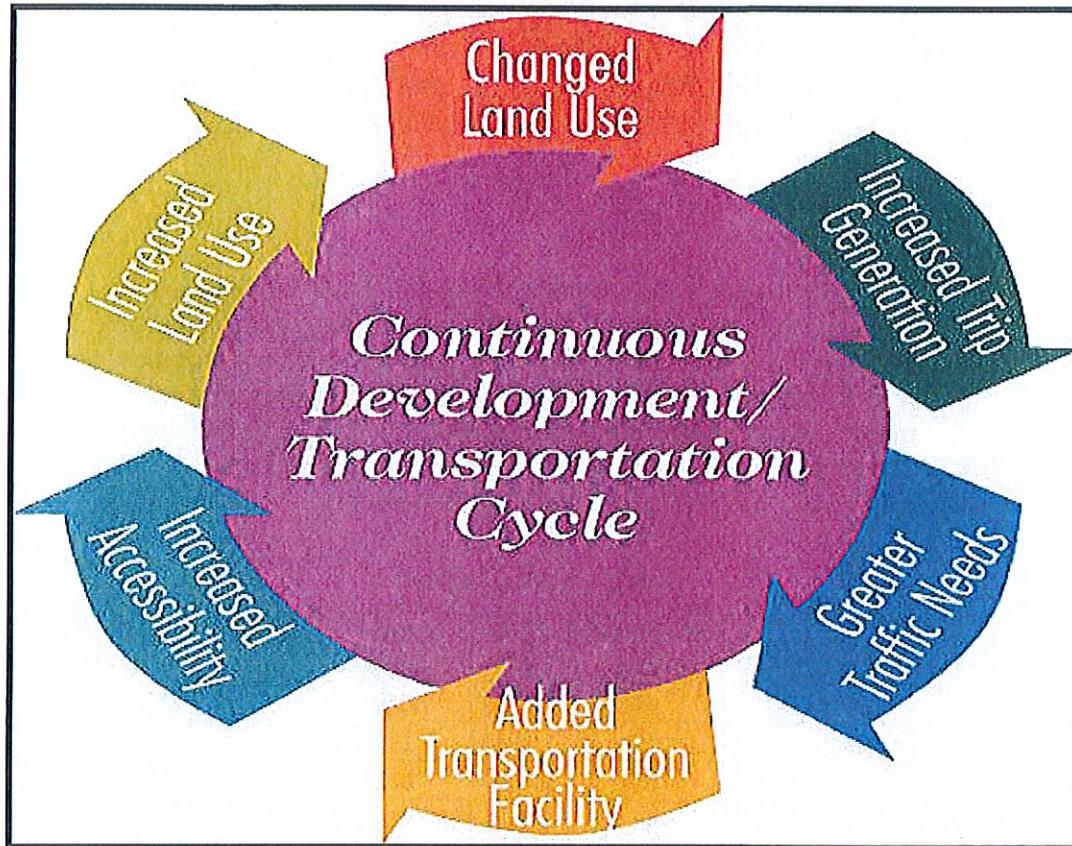
60th Avenue to 43rd Avenue

Adjacent land is currently developed as small to mid-size commercial. There is limited potential for redevelopment. From 45th Avenue to 43rd Avenue, it is anticipated to remain the same land use as today.

These future developments will compete with existing land uses for the same transportation facilities and demand access, safety and mobility for their users. Some types of facilities, more specifically residential and large employment centers, generate significant peak hour traffic and can influence neighboring land

uses and transportation characteristics. The site selections of these types of facilities require careful consideration of the future traffic signal placement and traffic progression along WIS 50. Figure 2 shows the continuous cause and effect relationship between land use and transportation.

Figure 2 - Land Use & Transportation Relationship



2.2 Increasing Traffic, Safety, and Operational Issues

Increasing Traffic

2008 traffic volumes on WIS 50 range from 24,800 Average Annual Daily Traffic (AADT) to 32,600 AADT.

Forecasted average volumes are anticipated to increase to between 9,300 and 13,450 AADT by 2039 under present trends in development. This represents an anticipated average increase of over 39 percent over the next 30 years. Projected traffic growth percentages for specific corridor segments are shown in Table 1.

Table 1 - Traffic Volumes

Location	2008 AADT	2039 AADT	% Growth
I-94 - 120th Ave	28300	39100	38%
120th Ave - 104th Ave	26400	36600	39%
104th Ave - 94th Ave	24800	36300	46%
94th Ave - STH 31	31700	45150	42%
STH 31 - 60th Ave	32600	45100	38%
60th Ave - 45th Ave	29300	38600	32%
45th Ave - 43rd Ave	27000	37900	40%

Safety

Crash Rate

Tables 2 and 3 summarize the crash data by dividing the mainline into 7 segments. Crash data is displayed for both 2005 (Table 2) and 2008 (Table 3). The crash rates were computed for the years 2005 and 2008 because these years correspond to the same years that actual mainline traffic volumes were counted. Crash rates were then compared to the statewide average crash rate for "large urban divided highways." Several of the segment crash rates exceeded the statewide average crash rates for similar urban corridors.

Table 2 – 2005 Crash Data

Segment	Length (miles)	2005 Crash Total	2005 AADT	Segment Crash Rate	% Above Statewide Urban Crash Rate Average of 326
I-94 - 120th Ave	0.12	40	29400	3106	853%
120th Ave - 104th Ave	0.82	11	27500	134	Below
104th Ave - CTH H	1.00	21	27100	212	Below
CTH H - STH 31	1.37	68	31300	434	33%
STH 31 - 60th Ave	0.36	23	33000	530	63%
60th Ave - 51st Ave	0.61	49	29700	741	127%
51st Ave - 43rd Ave	0.43	9	24250	236	Below

Table 3 – 2008 Crash Data

Segment	Length (miles)	2008 Crash Total	2008 AADT	Segment Crash Rate	% Above Statewide Urban Crash Rate Average of 326
I-94 - 120th Ave	0.12	21	28300	1694	420%
120th Ave - 104th Ave	0.82	20	26400	253	Below
104th Ave - CTH H	1.00	25	24800	276	Below
CTH H - STH 31	1.37	55	31700	347	6%
STH 31 - 60th Ave	0.36	20	32600	467	43%
60th Ave - 51st Ave	0.61	31	29300	475	46%
51st Ave - 43rd Ave	0.43	6	27100	141	Below

Access Points

Each access point creates potential conflicts between through traffic and traffic using that access. Each conflict is a potential crash. There is a relationship between the number of crashes and the number of access points according to a study conducted by the Federal Highway Administration (see Figure 3). According to the study, as the number of access locations increase along a highway, the rates of crashes also increase. Further, poorly located access either too close to intersections or inadequately spaced from other access points contributes to more crashes.

Figure 3 - Relationship between Access Points and Crash Rates

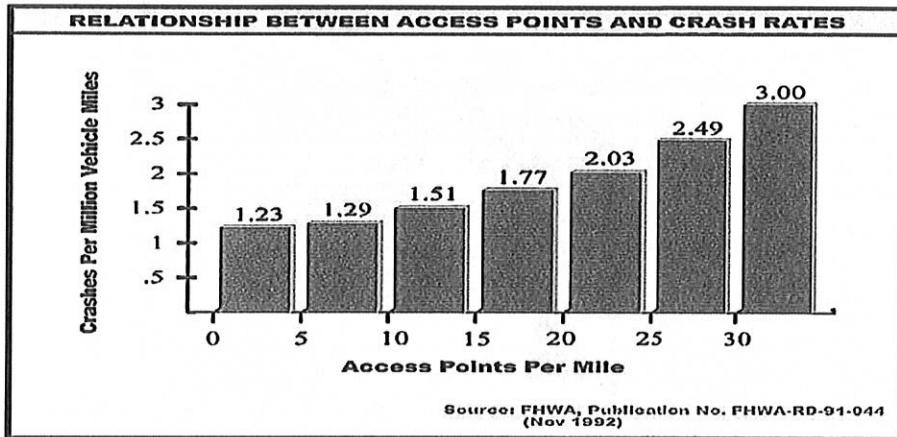


Figure 4 shows the introduction of 11 conflict points for every driveway introduced on a 4-lane roadway.

Figure 4 - Four Lane Roadway with One Driveway (11 conflict points)

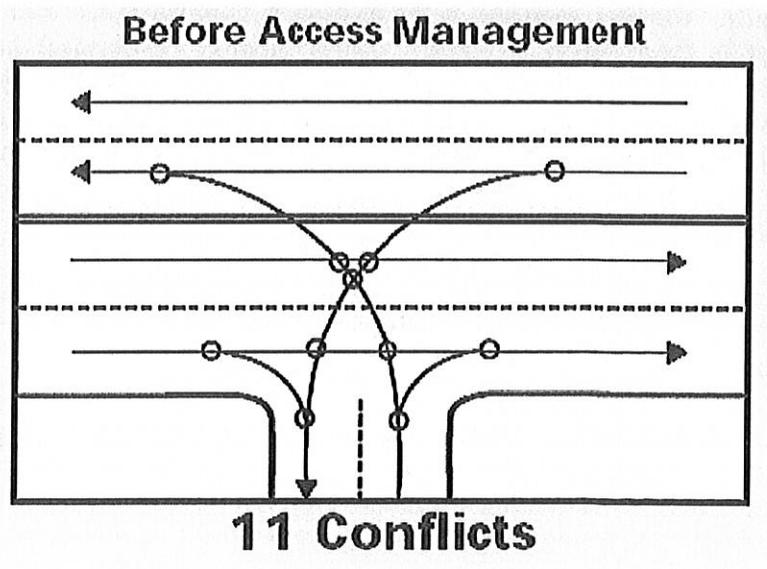
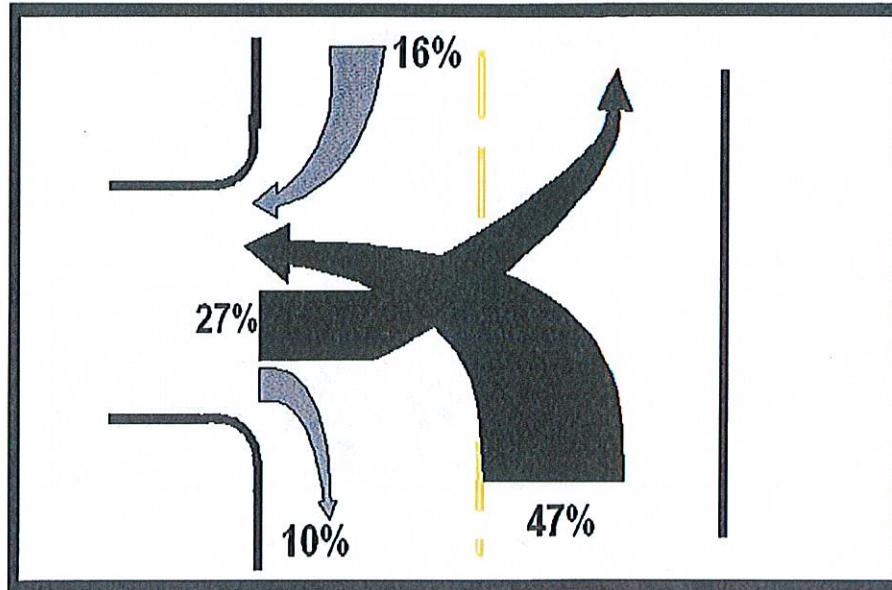


Figure 5 shows the percentage of driveway crashes per type of movement. Nearly half of all crashes occur when drivers attempt a left turn into the driveway if the mainline facility is undivided or has inadequate left turn storage. Over a quarter of the crashes occur maneuvering the left turn out of the driveway.

Figure 5 - Percent of Driveway Crashes by Movement



Eliminating the left-out maneuver from driveways requiring a right-out only and U-turn maneuver at the next available median opening has shown a 27% reduction in injury fatalities.

Operational Issues

As traffic volumes increase without proper improvements to the highway, congestion (user delay) and safety issues (number of crashes) increase. As congestion increases, driver frustration and perception that the facility is “difficult to drive” also increases. Drivers will change their “destination” to seek more convenient, faster or safer routes to obtain the same services. In addition to longer commute times, higher fuel consumption and pollutant emissions result from congested roadways.

Congestion also affects the efficiency of most commercial businesses. Delay and inconvenience to customers and deliveries affect the direct profits and success of most businesses.

If traffic congestion on the transportation network is not managed properly, the economic vitality of the area will decline and not be attractive to new developments. Some existing businesses may choose to economically disinvest in the corridor and seek higher quality transportation facilities and communities.

2.3 Improved Intergovernmental Coordination

Successful implementation of this Access Management Vision is dependent upon intergovernmental commitment to the “common vision” and coordination.

By permit authority, WisDOT will have the ability to remove, relocate or adjust any access or median opening if deemed unsafe to the transportation facility.

The local communities have permit authority to review and approve planned land use, re-developments and new developments. By permit authority, local communities will also have the ability to require cross-access easements and shared driveways conditions.

Both, local communities who control land use and WisDOT who is responsible for WIS 50 need to work together to establish and maintain a safe, efficient and economically viable corridor long into the future.

The key to success is to review the vision in combination as each development request or access request is received and come to a mutual intergovernmental agreement for any vision changes.

2.4 Common Long-range Corridor Vision

A function of the access management vision is to provide a “common vision” for this corridor. Since two communities lie within the corridor, having a common vision with the Department will insure a consistent application of access management principles that will provide regional mobility.

Regional mobility is important to the businesses adjacent to WIS 50 because their customers, deliveries and employees will be arriving from an array of regional origins, not any one particular community. A poorly planned congested area will impact upstream and downstream communities similar to a clog in a pipe.

Further, expectations and requirements of a developer should be consistent within a regional area, therefore making it important to the success of the vision that community decisions compliment the decisions made by other communities within the prevue of the “common vision.”

3.0 Access Management Techniques and Strategies

It is anticipated that elements of the vision would be implemented in an incremental approach over time as land uses and access needs change or when improvements are made to WIS 50.

Coordinated implementation of the techniques and strategies is the key component to the overall success of the vision. For some recommendations, such

as providing alternate routes for circulation, or allowing temporary access to side roads may fall under multiple local jurisdictions.

There are four primary areas of focus for the strategies included within the vision:

- Location of existing and future signalized intersections for traffic progression
- Location of un-signalized intersections (future median opening locations)
- Private access to WIS 50
- Local Network for circulation

3.1 Signalized Intersections and Traffic Progression

The demand for additional traffic signals usually increases as traffic volumes increase. In order to preserve mobility, maintain traffic progression, and meet intersection control needs, the optimum spacing of traffic signals should be spaced one-half mile apart. This spacing may be adjusted due to speed during the detailed design stage and will allow future signals to be interconnected so travel through the system will occur with fewer stops and delay. Progression is difficult to obtain with greater distances and operate undesirably for closely spaced intersections. Table 4 shows general guidelines for intersection spacing.

Table 4 - General Access Criteria

STH 50 Access Management Plan General Access Criteria For Retrofit Situations				
Intersecting Facility Type	Signal	Median Condition		
		Full Access	Restricted Access	Closed Access
Single Use Private Driveway	No	No	No	Allowable
Shared Private Driveway/Cross-Access	No	No	Allowable	Allowable
Private Road	No	No	Allowable	Allowable
Public Street - Cul-de-Sac (no circulation)	No	No	Allowable	Allowable
Public Street - Local	No	Allowable	Allowable	Allowable
Public Street - Collector	Allowable	Allowable	Allowable	No
Public Street - Arterial	Allowable	Allowable	No	No
	1/2 Mile	1/4 Mile	1/8 Mile	None
Generalized Spacing Criteria				

*Minimum distance from the functional area of a median opening is 200 ft.

*Minimum distance from the functional area of an intersection is 500 ft.

3.2 Un-Signalized Intersections (median opening locations)

The optimum location for un-signalized intersections is one-quarter mile (when half-mile is used for signal spacing). This spacing allows for the proper development of the intersection influence area that includes the storage bays for each turning movement.

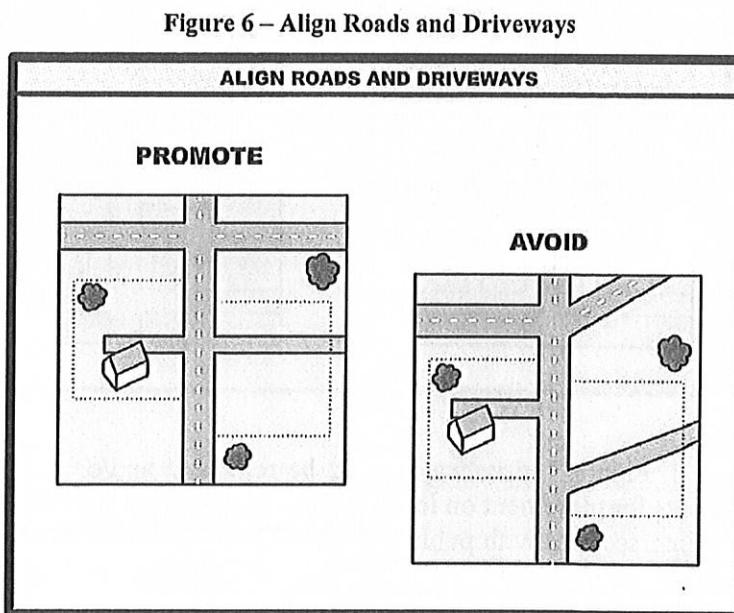
A four-leg intersection is desirable over offset T-type intersections because all movements onto local roads can be accommodated at one location more efficiently than at two closely spaced locations. As traffic increases, the offset T-type intersections experience a higher risk of crashes as the number of vehicles entering the intersection increases and close spacing restricts the proper length of turning storage bays. For this reason, as traffic increases on WIS 50 the only option available may be to limit one of the intersections to right-in/right-out movements if operational or safety problems arise.

Another consideration for intersection spacing is when the facility is expanded to a divided 4-lane facility. Proper intersection spacing will ensure proper median opening spacing and safe U-turn opportunities.

Strategies to address intersections with public streets include:

- Realign offset T-type intersections if possible.
- Relocate existing local road connections to optimum spacing guidelines where possible.
- Locate new local road connections at optimum spacing guidelines
- Require cross access easements for all properties between intersection locations

Figure 6 shows desired intersection and driveway alignment.



3.3 Private Access

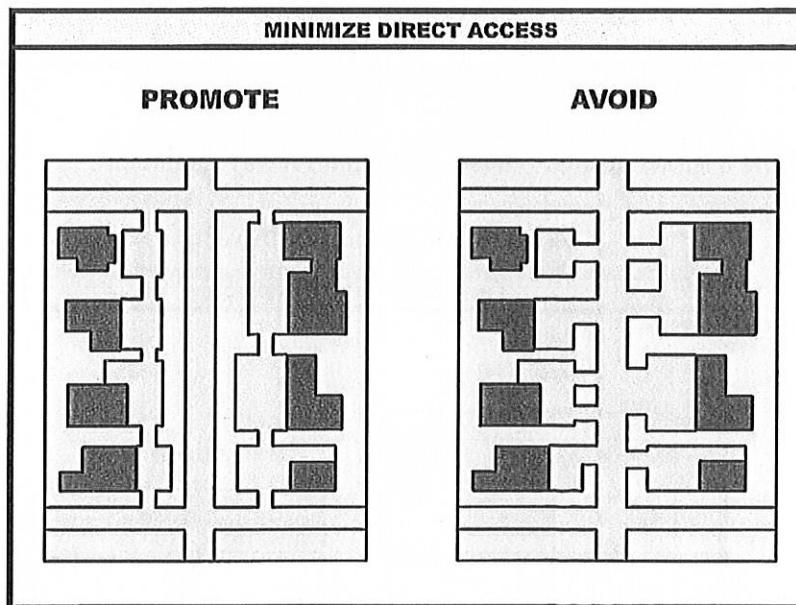
Direct access to WIS 50 should be minimized for the following reasons:

- Narrow lot sizes result in closely spaced driveways; large lot sizes often request multiple driveways.
- Median openings may not be allowed at driveways when facility is expanded.
- Left turn maneuvers into and out of driveways account for 75% of all crashes at a driveway.

Strategies to address numerous access points along WIS 50 include:

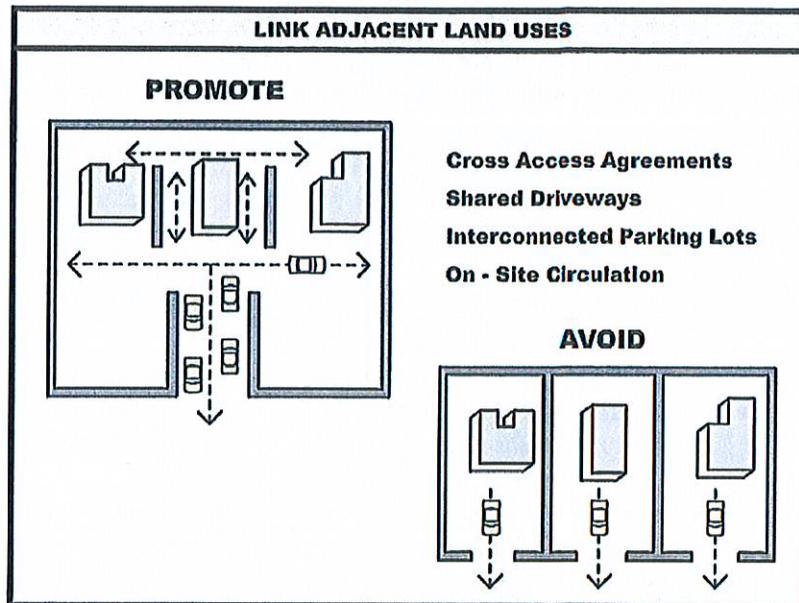
- Remove excess driveways for lots with multiple access points.
- Relocate driveways to increase spacing or to lower function roadways.
- Consolidate driveways for combined lots.
- Promote shared driveways between lots to consolidate large access points.
- Require cross access easements to enable multiple properties to use the same driveway.

Figure 7 - Minimize Direct Access



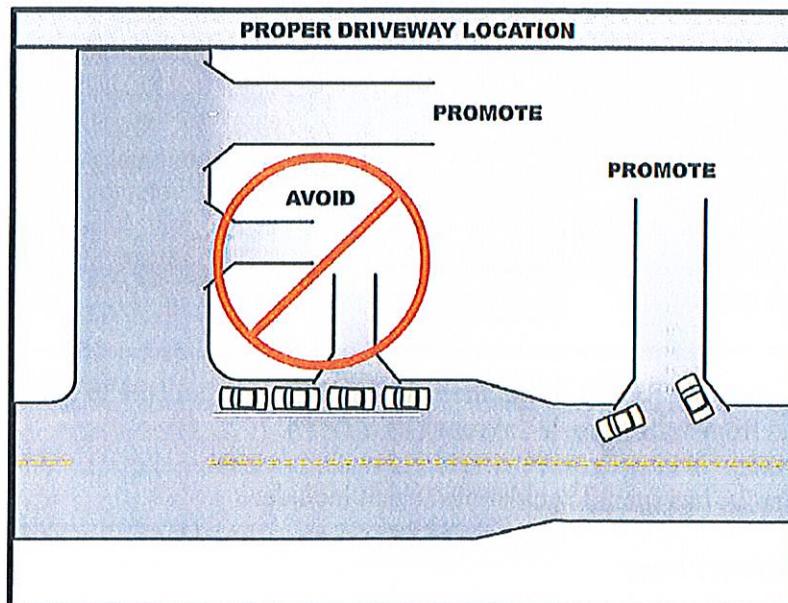
As shown in Figure 7, driveways could be relocated and/or promoted in future developments for placement on local roads. Access to the state highway would be limited to intersections with public streets. The reduction in the number of direct access points allows more efficient use of roadway and promotes greater separation of intersections.

Figure 8 - Link Adjacent Land Uses



In the case of shared-access or cross-access agreements, multiple parcels share a common access point. Access to each property is achieved through an interconnected parking lot system or other method of on-site circulation (see Figure 8). This solution reduces direct access driveways if the creation of new local roads is not possible. The movement of circulation off of the state highway and onto adjacent properties is achieved through local zoning and subdivision ordinances.

Figure 9 - Proper Driveway Location



Driveways located close to an intersection can pose safety and operational issues for the intersection (see Figure 9). These driveways should be relocated further from the intersection where possible to eliminate conflicts between vehicles entering the intersection and those using the driveway. These driveways can pose safety hazards from vehicles making left turns into the driveways too close to the intersection. In addition, it may be difficult for drivers to determine if a vehicle is entering the intersection to execute a turn, or if the vehicle is entering the driveway. For vehicles wishing to exit from a driveway that is too close to the intersection, they often must track several movements within the intersection to determine a safe opportunity to make a left-turn out of the driveway.

The state’s Access Management Plan (SAMP), categorizes the state trunk highways into five access tiers (1, 2A, 2B, 3, and 4). The SAMP provides guidelines of future access management goals for each state trunk highway.

The five access types are defined as ranging from a high degree of access control or a tier 1 (state trunk highways that maximize Interstate or statewide mobility goals, such as I-94) to state trunk highways with a balance between mobility and access on tier 4 routes (for example, WIS 32 from WIS 100 to the City of Milwaukee). Guidelines for access types are outlined on Table 5.

Table 5 - SAMP Tier Guidelines

Goal for Access and Traffic Movement	Type of New Access Allowed
Tier 1 - maximizes Interstate/Statewide traffic movement	<ul style="list-style-type: none"> • Interchanges • Locked/gated driveways for emergency vehicles • On an interim basis – isolated field entrances
Tier 2A - maximizes Interregional traffic movement	<ul style="list-style-type: none"> • At-grade public road intersections, with some interchanges possible at higher volume routes • Locked/gated driveways for emergency vehicles • On an interim basis – isolated entrances
Tier 2B – maximizes Interregional traffic movement	<ul style="list-style-type: none"> • At-grade public road intersections • Lower volume residential, commercial, and field
Tier 3 - maximizes Regional/Intra-urban traffic Movement	<ul style="list-style-type: none"> • At-grade public road intersections • Higher volume residential, commercial, and field
Tier 4 - Balances traffic movement and property Access	<ul style="list-style-type: none"> • All types, provided they meet safety standards

The current SAMP shows the existing WIS 50 from I-94 to WIS 31 as a Tier 2A and from WIS 31 to 43rd Avenue as a Tier 4.

Tier 2A has specific guidelines which include:

Existing Access:

-
- Access primarily by at-grade public intersections, with some interchanges possible at higher volume roads.
 - Existing widely spaced, extremely low volume (generally < 50 AADT) private, residential/field access or emergency service may be allowed, if no reasonable alternative or opportunity to obtain such exists, and a long-term plan is in place for removing existing access as opportunities arise.

New Access:

- Access primarily by at-grade public road intersections, with some interchanges possible at higher volume roads.
- No at-grade intersections within one mile of interchange entrance ramps.

Tier 4 has specific guidelines which include:

Existing and New Access:

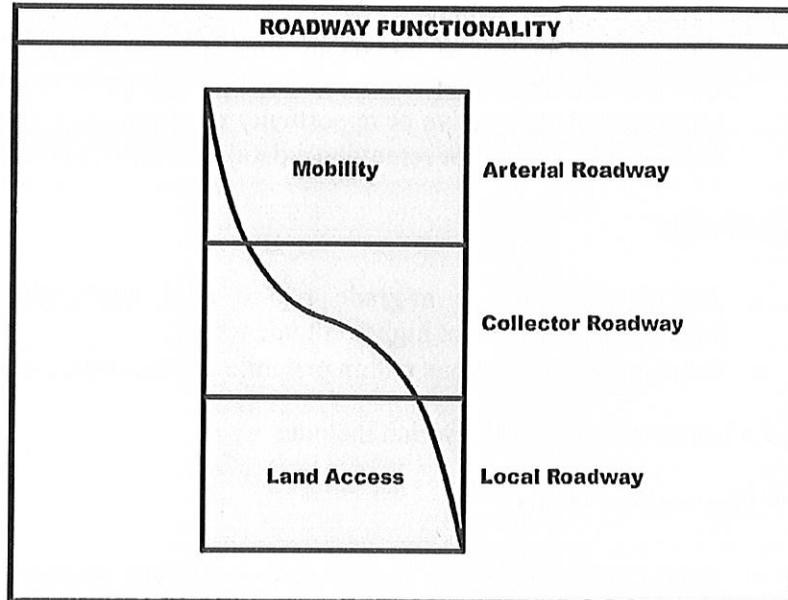
- Public and private access roads spaced for safe operation, and meeting departmental/operational standards for safety.

3.4 Local Circulation

The transportation system is designed to accommodate the existing land use. As investments are made to change the land use, investments need to be made to expand the transportation infrastructure to support the new land use.

A local transportation system should be developed that is balanced according to the desired function of the roadway. There are three primary functions that roadways perform including providing mobility, providing access, or acting as a transition between roadways that provide access and those that provide mobility (see Figure 10).

Figure 10 - Roadway Functionality



Roadways that provide the function of mobility are classified as arterial roadways. These roadways have minimal direct access and operate at higher speeds for mobility (45 mph or greater).

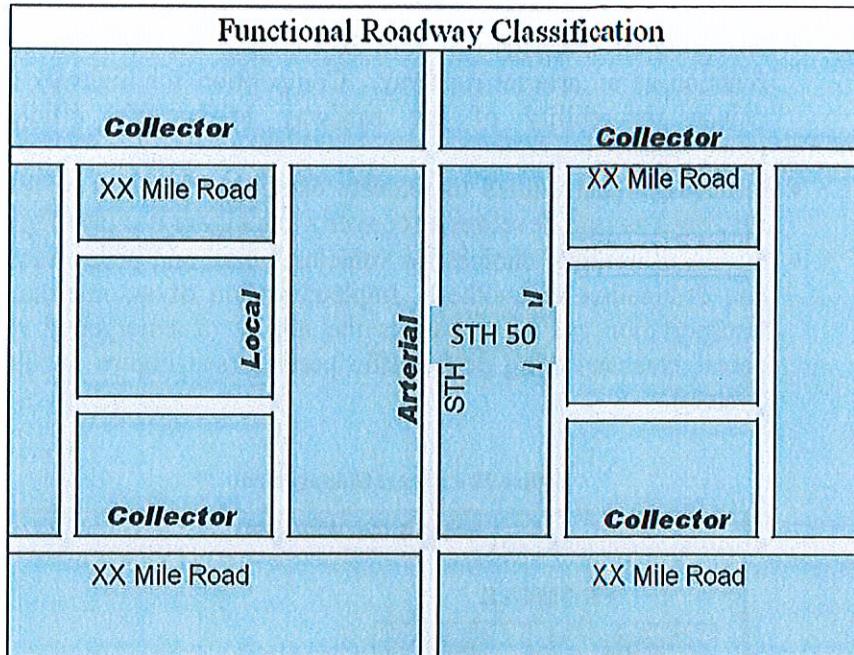
Roadways that provide the transitional function between land accessibility and mobility are classified as collector roadways. These roadways allow more access than arterials and operate at lower speeds than arterials (35 to 40 mph).

Roadways that provide the access function are classified as local roadways. These roadways allow access to adjacent properties and operate at low speeds (25 to 30 mph).

The most desired transportation network needs to satisfy both functions and thereby needs to have all three types of roadways for system that will operate efficiently and safety long into the future.

If all three types of roadways are not in a network, then the roadways do not operate as intended and degraded operations prevail. Examples of degraded facilities are plenty and if possible are costly to remedy. In most cases, the length of congested periods increase annually and safety and inconvenience issues expand.

Figure 11 - Functional Roadway Classification



As Figure 11 shows, WIS 50 should operate as an arterial roadway allowing minimal access and traffic operation at high speeds of 45 to 55 mph. Therefore, the consideration where possible to plan and develop a local road network will satisfy future land use changes and will compliment the collector and arterial system already in place in the corridor.

The local road system also provides alternative routes for greater accessibility to land use in the region. The recommendations pertaining to local circulation recognize that this issue lies outside of WisDOT jurisdiction and falls under local community jurisdiction.

The local circulation routes presented in this vision fall under one of two strategies:

- Local circulation routes shown on the vision are conceptual recommendations rather than location specific. For example, local circulation that provides parallel connectivity for short trips should be provided if possible. However, the exact method (cross-access agreement or new public road) and the exact location of those routes need to be determined by the respective local unit of government.
- Connections of proposed local circulation routes with WIS 50 and/or the existing local street system are location specific. These connections are shown in their recommended locations on the vision.

The benefits of providing a local transportation system include:

- Providing an alternative to WIS 50 for local trips will enable WIS 50 to function as an arterial roadway. Competition for highway capacity will reduce the ability of the roadway to provide efficient regional transportation.
- Providing opportunities to relocate existing driveways to lower function roadways as land develops or converts to a higher use (see Figure 12).
- Providing multiple choices for vehicles, bikes, and pedestrians to get from one destination to another. Implementation of an interconnected local transportation system enhances the ability of emergency responders to access crashes during peak traffic periods (see Figure 13, Transportation System).

Figure 12 - Access Management

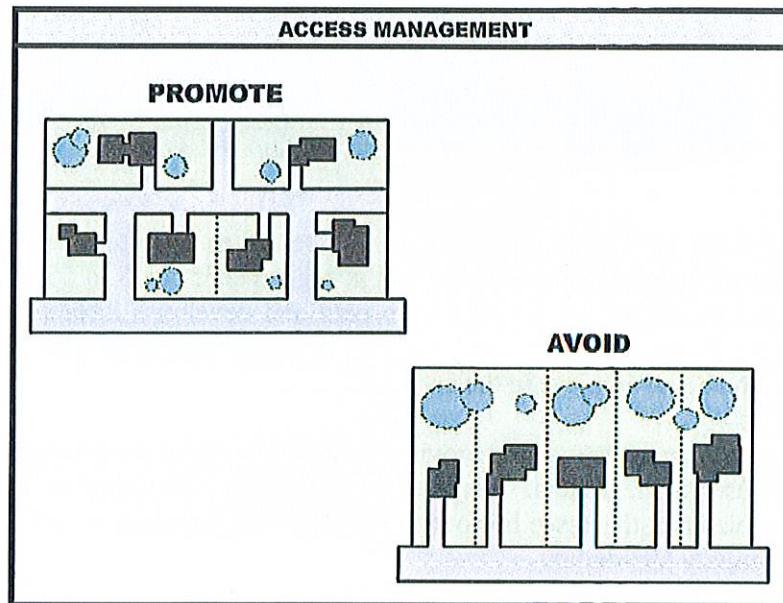
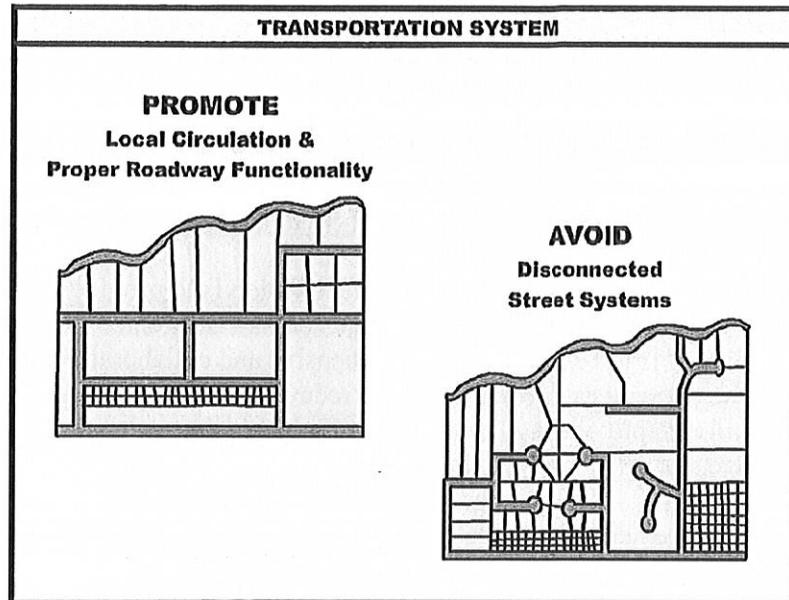


Figure 13 - Transportation System



In summary, minimizing the number of curb cuts, consolidating driveways, constructing landscaped medians (where appropriate), and coordinating internal site circulation and parking among several businesses results in a visually pleasing and more functional corridor. Using these techniques will protect the investment of existing businesses, the public investment in the roadway and can help attract new investment into the area.

4.0 Memorandum of Understanding

The signing of the "Memorandum of Understanding" acknowledges the participation of all communities in the development of the plan and signifies a "good faith partnership" to work together as future land use, access and transportation decisions need to be made along WIS 50.



Division of Transportation
 System Development
 Southeast Regional Office
 141 N.W. Barstow Street
 P.O. Box 798
 Waukesha, WI 53187-0798

Scott Walker, Governor
 Mark Gottlieb, Secretary
 Internet: www.dot.wisconsin.gov

Memorandum of Understanding

RE: WIS 50 Access Management Vision (I-94 to 43rd Avenue)

January 2012

The purpose of the WIS 50 Access Management Vision is to provide WisDOT and the local units of government a unified, long-range access management vision of the corridor. The Vision's intent is to be used as a comprehensive and collaborative tool for evaluation of future access requests as development and redevelopment occurs adjacent to WIS 50. The Vision will also provide a guide to local officials to determine the optimum location for developments of differing type, scale and traffic generation.

The Access Management Vision includes strategies and recommendations to:

- Managing existing access
- Managing new access due to new or changing land use
- Managing traffic safety and efficiently as traffic growth occurs
- Planning a supportive local road network
- Improving coordination and cooperation between WisDOT and local units of government

The Vision's development was a result of a collaborative effort with the local communities listed below:

Kenosha County Village of Pleasant Prairie City of Kenosha

By signing this document, your community is agreeing to partner with WisDOT and the other local units of government to implement this Vision whenever possible as future opportunities arise.

Name

Title

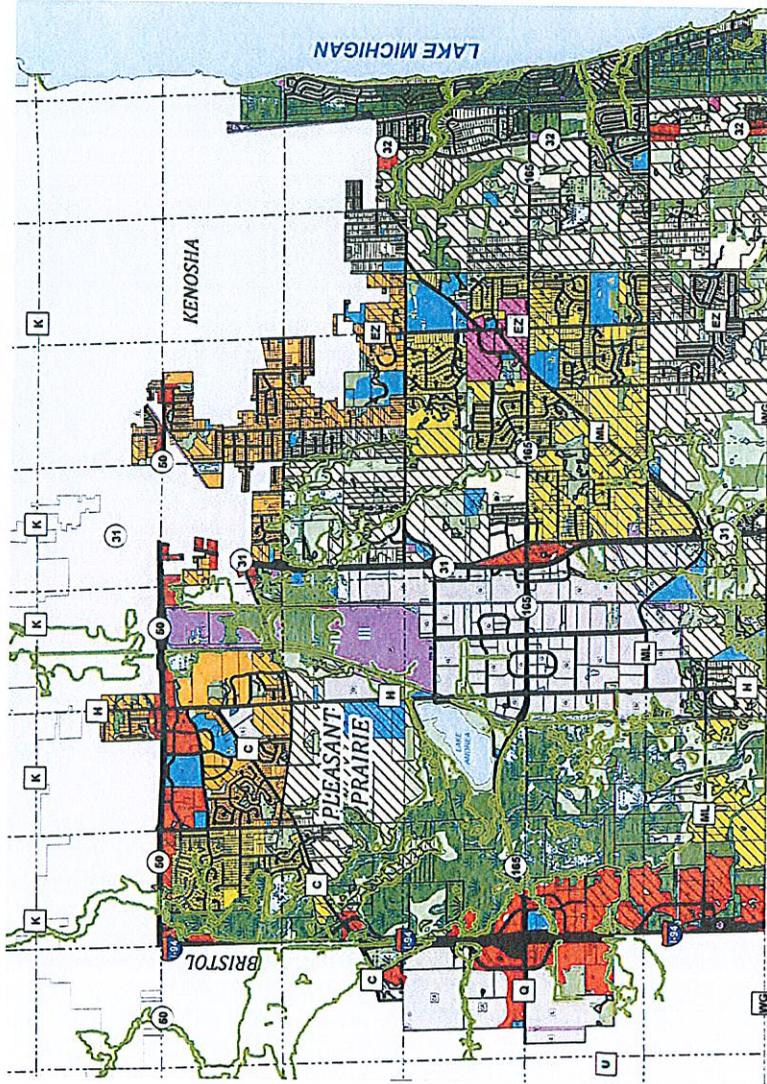
Date

5.0 Appendices

5.1 2035 Land Use Plan Maps

5.2 Access Management Project Maps

**MAP 9.9
VILLAGE OF PLEASANT PRAIRIE 2035 LAND USE PLAN**

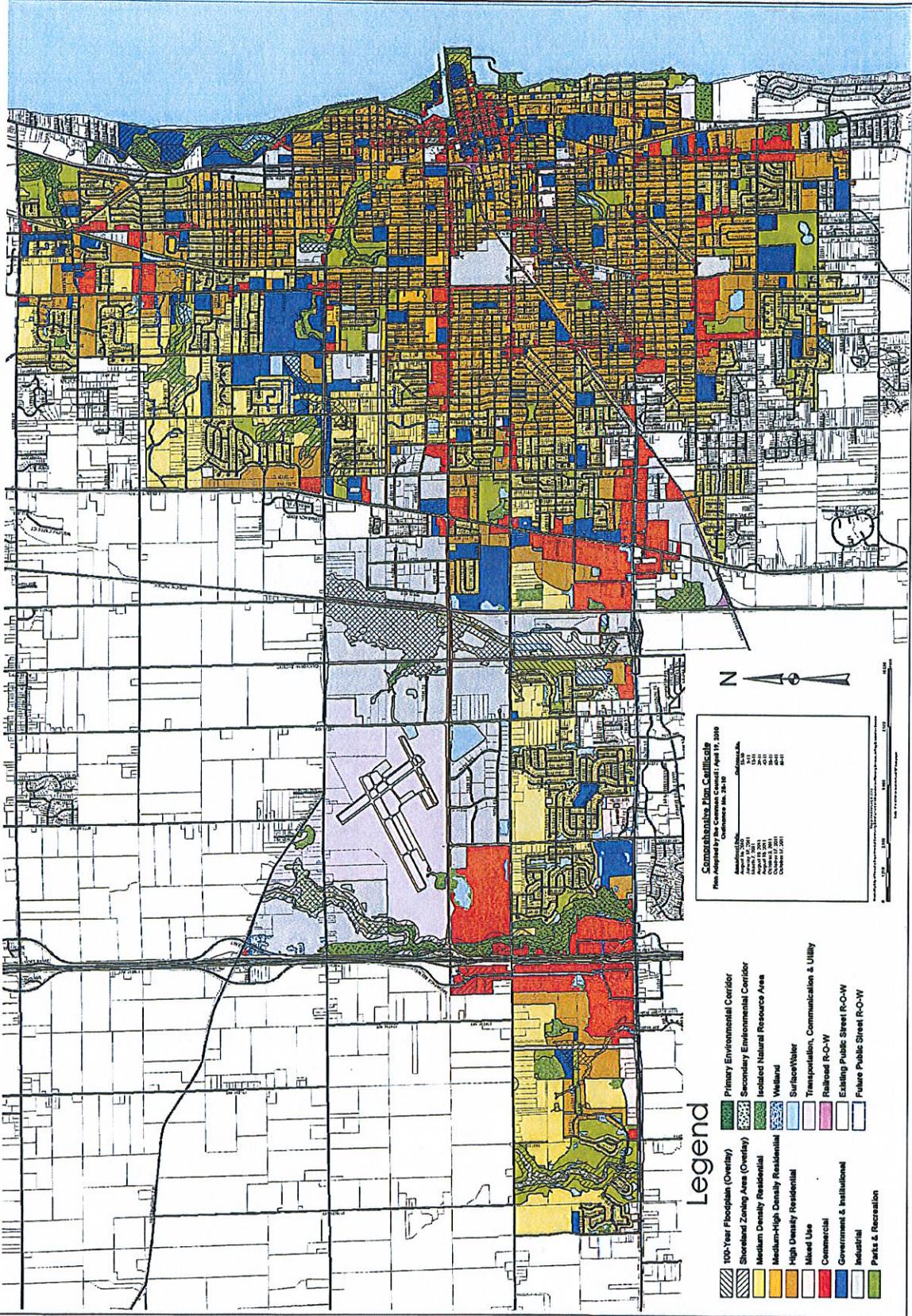


Note: See Appendix 10-3 for Amendments to this Map

Source: Village of Pleasant Prairie

- RESIDENTIAL LANDS**
- LOW DENSITY RESIDENTIAL (10,000 Square Feet Or More Per Dwelling Unit)
 - LOW-MEDIUM DENSITY RESIDENTIAL (12,000 To 16,999 Square Feet Per Dwelling Unit)
 - UPPER-MEDIUM DENSITY RESIDENTIAL (6,200 To 11,999 Square Feet Per Dwelling Unit)
 - HIGH DENSITY RESIDENTIAL (Less Than 6,200 Square Feet Per Dwelling Unit)
- MIXED USE LANDS**
- MG Village Green Center
 - CT Characteristic Transit Center
- COMMERCIAL LANDS**
- CA General Office and Service Centers
 - C Community Retail and Service Centers
 - F Freeway-Oriented Service Centers
 - B Business Office and Professional Centers
 - BA-1 Business Area 1 Sub-District
 - BA-2 Business Area 2 Sub-District
 - BA-3 Business Area 3 Sub-District
- OTHER TRANSPORTATION, COMMUNICATIONS AND UTILITIES**
- TR Transit
 - U Utility
 - AV Active Landfill
 - RR Railroad
 - HT Highway
 - Y Overhead High Tension Wires
- GOVERNMENTAL AND INSTITUTIONAL LANDS**
- HA Municipal Administrative Offices
 - PS Public Storage
 - H Hospital
 - IS Intermediate School
 - HS Public Middle School
 - SP Public Senior High School
- PARKS, RECREATION AND OPEN SPACE LANDS**
- MS Municipal Park
 - C Community Park
 - NS Neighborhood Park
 - RI Recreational Park
- INDUSTRIAL LANDS**
- GI General Industrial
 - CA PUD-1 Care Area Sub-District
- AGRICULTURAL LANDS**
- ENVIRONMENTALLY SIGNIFICANT AREAS**
- Primary Environmental Corridors
 - Secondary Environmental Corridors
 - Indicated Natural Resource Areas
 - Surface Water
 - Field Verified Wetlands
 - Indicated Wetlands (2005 Wetland Inventory Maps)
 - 100 - Year Floodplain
- RIGHTS-OF-WAY**
- Existing Right-of-Way (September, 2009)
 - Proposed but not Constructed Right-of-Ways (September, 2009)
- URBAN RESERVES (Overlay)**
- LINEAR FEATURES**
- Property Lines

Map IX-6
 Adopted Land Use Plan for the City of Kenosha Planning Area: 2035



Legend

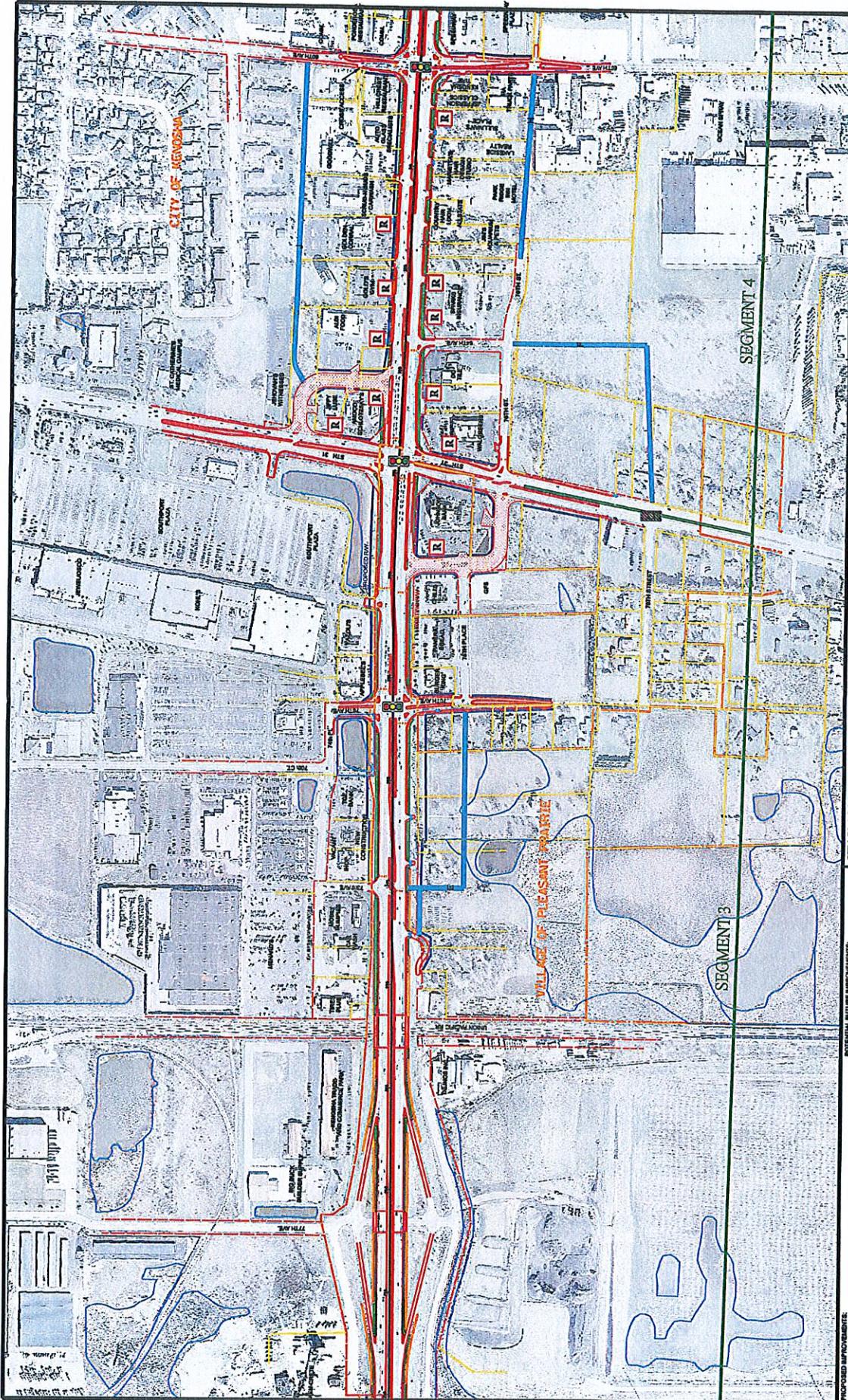
- 100-Year Floodplain (Overlay)
- Shoreland Zoning Area (Overlay)
- Medium Density Residential
- High Density Residential
- Commercial
- Industrial
- Parks & Recreation
- Primary Environmental Corridor
- Secondary Environmental Corridor
- Isolated Natural Resource Area
- Wetland
- Surface/Water
- Transportation, Communication & Utility
- Railroad R-O-W
- Existing Public Street R-O-W
- Future Public Street R-O-W
- Mixed Use
- Government & Institutional

Comprehensive Plan, Certificate
 Plan Adopted by the Common Council / April 19, 2010
 Ordinance No. 78-10

Revised by:
 January 2011
 February 2011
 August 2011
 October 2011



Scale: 1" = 100'



PROPOSED IMPROVEMENTS:

- PROPOSED RECONSTRUCTION
- PROPOSED IMPROVEMENTS
- POTENTIAL LOCAL ACCESS ROAD (PUBLIC OR PRIVATE)
- POTENTIAL FUTURE SIGNAL (POTENTIAL SIGNAL WARRANTS ARE MET)

POTENTIAL FUTURE IMPROVEMENTS:

- POTENTIAL LOCAL ACCESS ROAD (PUBLIC OR PRIVATE)
- POTENTIAL FUTURE SIGNAL (POTENTIAL SIGNAL WARRANTS ARE MET)

STH 50 ACCESS MANAGEMENT VISION
 STH 50 (WITH STREET CORRIDORS STUDY)
 104 TO 109 AVENUE
 KEVOSHA COUNTY
 PROJECT NO. 2209 03 00
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 JAN 2012



POTENTIAL LOCAL ACCESS ROAD (PUBLIC OR PRIVATE)

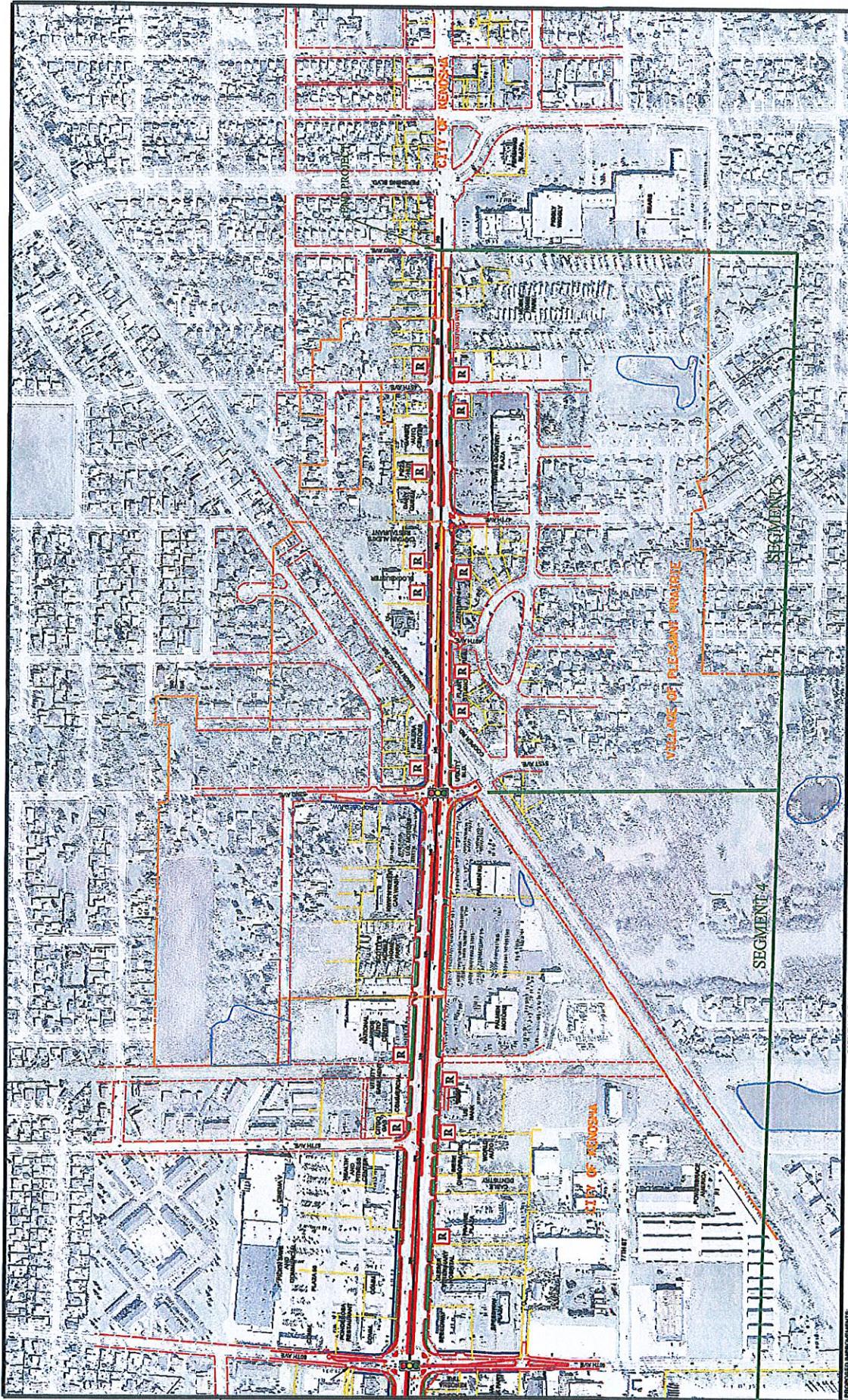
POTENTIAL FUTURE SIGNAL (POTENTIAL SIGNAL WARRANTS ARE MET)

POTENTIAL LOCAL ACCESS ROAD (PUBLIC OR PRIVATE)

POTENTIAL FUTURE SIGNAL (POTENTIAL SIGNAL WARRANTS ARE MET)

WATER

CHANNING



LEGEND

- PROPOSED IMPROVEMENTS
- PROPOSED ADVANCEMENT
- CURRENTLY FINANCED UNDER STH 50 RECONSTRUCTION

POTENTIAL FUTURE IMPROVEMENTS:

- EXISTING SIGNAL
- POTENTIAL LOCAL ACCESS ROAD (PUBLIC OR PRIVATE)
- POTENTIAL LOCAL ACCESS ROAD (PUBLIC OR PRIVATE)
- FUTURE CORRIDOR CLOSURE (POTENTIAL LOCAL ACCESS ROAD)
- FUTURE CORRIDOR CLOSURE (POTENTIAL LOCAL ACCESS ROAD)
- POTENTIAL IMPROVEMENT ON LAND USE CHANGE(S)

STH 50 ACCESS MANAGEMENT VISION
 8TH ST (8TH STREET) CORRIDOR STUDY
 144 TO 4TH AVENUE
 KEVOSHA COUNTY
 PROJECT NO. 2258-03-00

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 JAN 2022

